

#8308

AS BUILT

SR 5
NE 145TH STREET INTERCHANGE

Washington State Department of Transportation
Olympia, Washington 98504

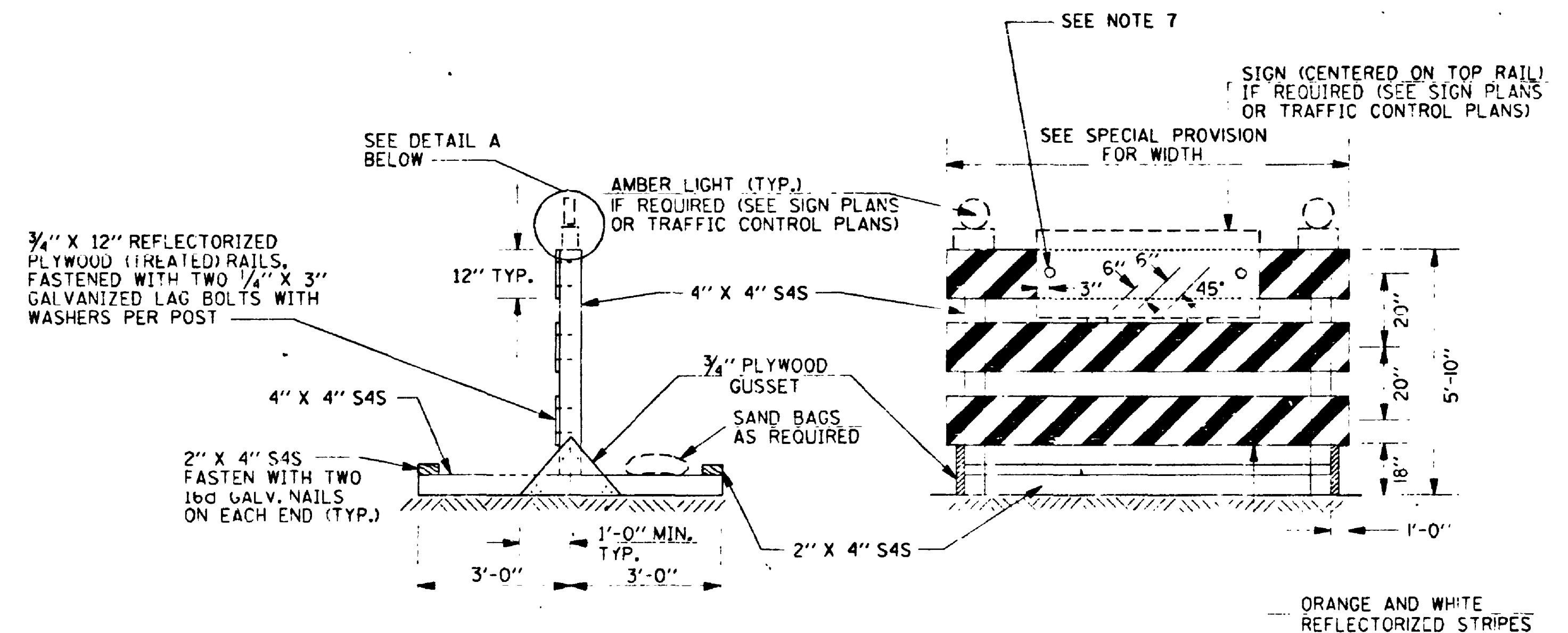
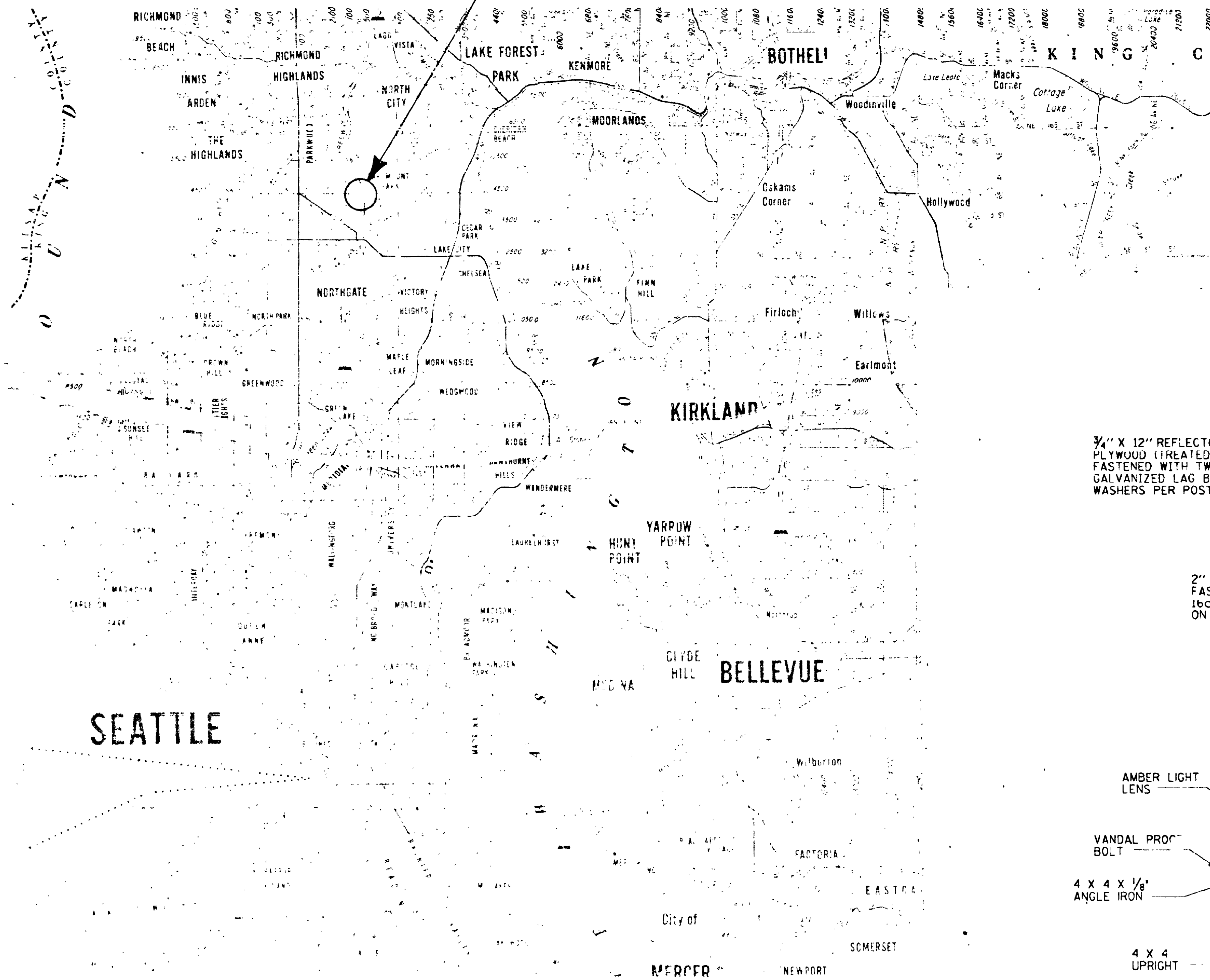
| | |
|---------------------------------|---------------------------------------|
| Scale: Horiz. 1 inch = 100 feet | Vert. 1 inch = 10 feet |
| MP | to MP |
| Contract No. | F.A. No. |
| Work Begun | 19__ |
| Project Engineer | Approved |
| | 19__ |
| | Work Completed |
| | Contractor |
| | 19__ |
| | District Transportation Administrator |
| | 19__ |

Secretary of Transportation

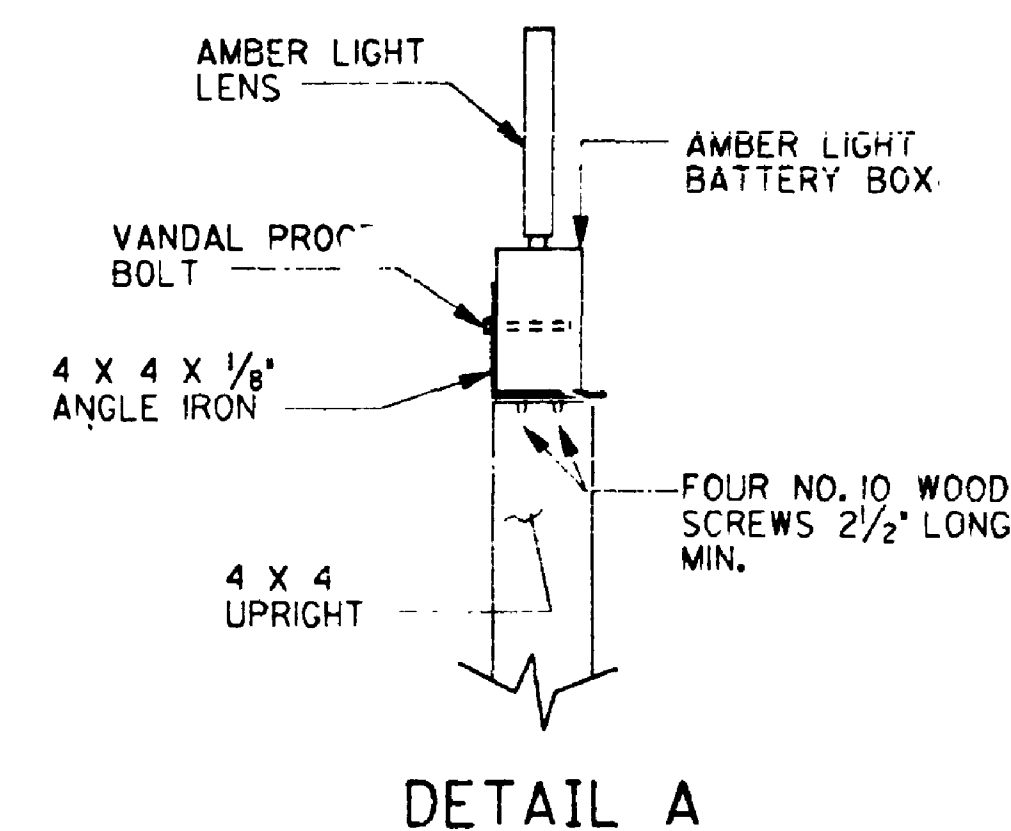
8308
3643

3643

IR-IRG-5-3(622)
PROJECT LOCATION
MP 174.58



SIDE VIEW
FRONT VIEW
TYPE III BARRICADE DETAILS



DETAIL A

- NOTES:
1. BARRICADES SHALL BE INSTALLED IN ACCORDANCE WITH PART VI OF THE MUTCD.
 2. BARRICADES SHALL CONFORM TO THE BASIC CONCEPTS AND DIMENSIONS AS SHOWN.
 3. PANELS SHALL BE MARKED ON BOTH SIDES AS REQUIRED.
 4. BARRICADES WITH STRIPES WHICH BEGIN AT THE UPPER RIGHT SIDE AND SLOPE DOWNWARD TO THE LOWER LEFT SIDE ARE TO BE DESIGNATED AS 'RIGHT' (R) BARRICADES. BARRICADES WITH STRIPES WHICH BEGIN AT THE UPPER LEFT SIDE AND SLOPE DOWNWARD TO THE LOWER RIGHT SIDE ARE TO BE DESIGNATED AS 'LEFT' (L) BARRICADES.
 5. MARKINGS FOR BARRICADE RAILS SHALL BE 1/2 INCH ORANGE AND WHITE STRIPES (SLOPING DOWNWARD AT AN ANGLE OF 45 DEGREES IN THE DIRECTION TRAFFIC IS TO PASS).
 6. WHERE A BARRICADE EXTENDS ENTIRELY ACROSS A ROADWAY, THE STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION TOWARD WHICH TRAFFIC MUST TURN IN DETOURING. WHERE BOTH RIGHT AND LEFT TURNS ARE PROVIDED FOR, THE CHEVRON STRIPING SHALL SLOPE DOWNWARD IN BOTH DIRECTIONS FROM THE CENTER OF THE BARRICADE.
 7. IF SIGN IS REQUIRED FIELD DRILL TWO HOLES TO ACCOMMODATE 3/8 IN. MACHINE BOLT, USE WASHER WITH NUT. THE SIGN SHALL BE INSTALLED IN A MANNER THAT THE CENTER CROSS RAIL IS NOT COVERED.

| | | | | | | |
|-------------|-----------------|--------------|-------|--------------------|-----------|--------------|
| ENGINEER | DANIEL TANSKY | NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| DRAWN | JIM OLSEN | 10 | WASH | IR-IRG-5-3(622) | | |
| REVIEWER | JOHN KERR | JOB NUMBER | | | | |
| CHECKED | HARRY ROLLINS | 89 W041 | | | | |
| PROJ. ENGR. | CLIFF KURTZWEIG | CONTRACT NO. | | | | |
| DIST. ADM. | RON BOCKSTROM | 3643 | | | | |
| DATE | REVISION | BY | APPD | | | |

HIGHWAY DIVISION
ALL SHEET OF THESE PLANS SHALL BE DELIVERED APPROVED BY THE SIGNATURE AND STAMP ON THIS SHEET
APRIL 28, 1989
APPROVED

Washington State
Department of Transportation

FOR "75" DISCLOSED
PLANS

SR 5
NE 145TH STREET INTERCHANGE

VICINITY MAP AND DETAILS

SHEET
1
THROUGH
24
SHEETS

89W041/05


NO LENGTH.

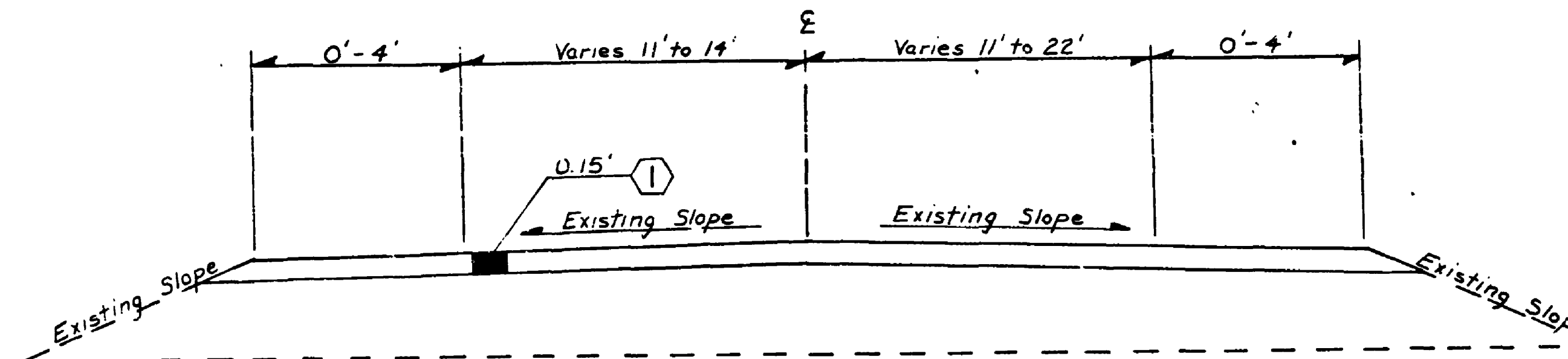
M 174.58

| IR-5-J(622) STATE BI FUNDS | | | | | | IRG-5-3(622) | | | |
|-------------------------------|-----------------------------|------------------------------------|---------------------------|---------------------------|--|------------------|-------------------------------|---|--|
| SR 5 | | | | | | SR 5 | | | |
| GROUP 1 | | | GROUP 2 | | | GROUP 3 | GROUP 4 | | |
| NE 145TH WEST ROADWAY | NE 145TH EAST ROADWAY | 5TH AVE. NORTHBOUND OFF-RAMP | SAFETY WEST ROADWAY | SAFETY EAST ROADWAY | SAFETY 5TH AVE. NORTHBOUND OFF-RAMP | BRIDGE 513/32 | SOUTHBOUND RAMPS SIGNAL | NORTHBOUND RAMPS 5TH AVE. SIGNAL | |
| CS 179200 | CS 179200 | CS 179200 | CS 179200 | CS 179200 | CS 179200 | CS 179200 | CS 179200 | CS 179200 | |
| L.S. | L.S. | L.S. | L.S. | L.S. | L.S. | L.S. | L.S. | L.S. | |
| 15 | 59 | | | | | | | | |
| 40 | | | | | | | | | |
| | 48 | | | | | | | | |
| | 142 | | | | | | | | |
| 183 | 540 | | | | | | | | |
| | 26 | | | | | | | | |
| 5 | 20.1 | | | | | | | | |
| L.S. | L.S. | | | | | | | | |
| | | | | | | | | | |
| | 4 | | | | | | | | |
| | | | | | | | | | |
| 10 | 10 | | | | | 40 | | | |
| 161 | 161 | | | | | 1,605 | | | |
| | | | | | | EST. 4,000 | | | |
| | | | | | | | | | |
| | 3 | | | | | | | | |
| EST. 10 | EST. 20 | EST. 230 | | | | | | | |
| | | | | | | | | | |
| 161 | 392 | 3,376 | | | | | | | |
| 16 | 66 | 368 | | | | | | | |
| | | | | | | | | | |
| 40 | 55 | | | | | | | | |
| | | | 183 | 540 | | | | | |
| | | | 225 | 499 | 745 | | | | |
| | | | 137 | 151 | 260 | | | | |
| | | | | 280 | | | | | |
| | | | 198 | 177 | | | | | |
| | | | 80 | 35 | 27 | | | | |
| | | | 2 | 2 | 6 | | | | |
| | | | | 4 | 8 | | | | |
| | | | .06 | .84 | 1.51 | | | | |
| | | | .04 | .7 | | | | | |
| | | | L.S. | L.S. | | | | | |
| | | | L.S. | L.S. | | | | | |
| | | | | | | | L.S. | L.S. | |
| | | | | | | | L.S. | L.S. | |
| | | | | | | | L.S. | L.S. | |
| | | | 100 | 100 | | | | | |
| | | | 5 | 6 | | | | | |
| | | | EST. | EST. | EST. | | | | |
| | | | 180 | 300 | 75 | | | | |
| | | | EST. | EST. | EST. | | | | |
| | | | L.S. | L.S. | | | | | |
| | | | 40 | 60 | | | | | |
| | | | | | | | | | |
| 27 | 65 | | | | | | | | |
| 2 | 2 | | | | | | | | |
| EST. 500 | EST. 500 | EST. 200 | | | | | | | |
| | | | L.S. | | | | | | |
| | | | L.S. | EST. 920,87 | | | | | |
| | | | L.S. | L.S. | | | | | |
| | | | | | | 2 | | | |
| | | | | | | L.S. | | | |
| | | | L.S. | | | | | | |

APPROXIMATE QUANTITIES ESTIMATED
AS BUILT QUANTITIES FROM RECORD DOCUMENTS

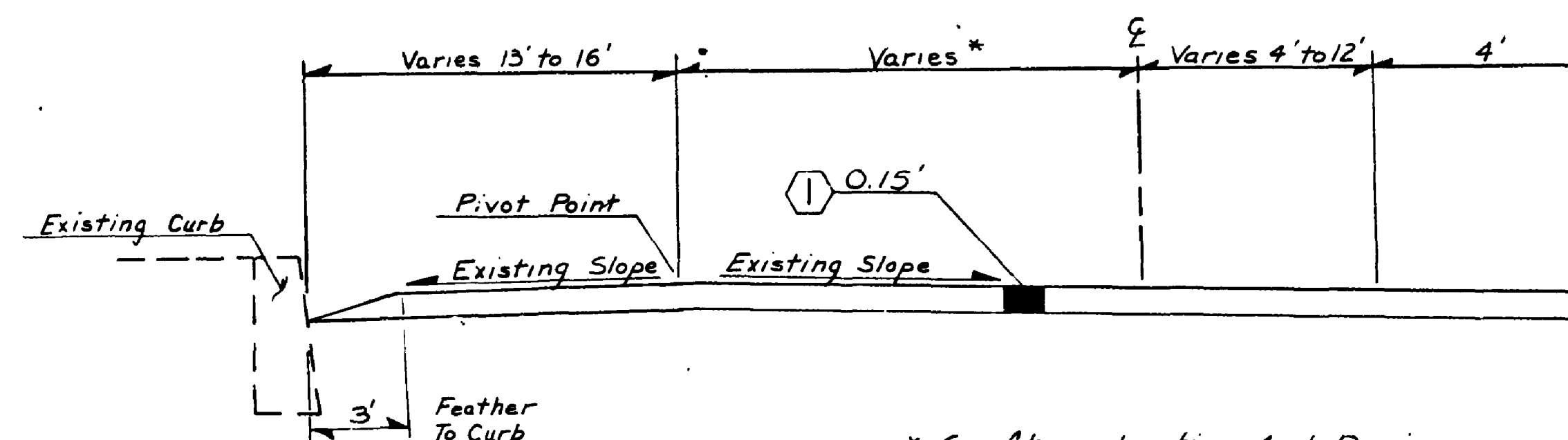
FOR "AS CONSTRUCTED
PLANS" ONLY

| | | | | | | | |
|-------------|--|----------------------|-------|---------------------|--|-----------------------------|----------------------------------|
| DESIGNED BY | | REGION NO. | STATE | FED. AID. PROJ. NO. |  Washington State Department of Transportation | SR 5 | SHEET 2 OF 24 SHEETS |
| ENTERED BY | | 10 | WASH | IR-5-3(622) | | NE 145TH STREET INTERCHANGE | |
| CHECKED BY | | JOB NUMBER 89W041 | | IRG-5-3(622) | | SUMMARY OF QUANTITIES | |
| PROJ. ENGR. | | CONTRACT NO. | | | | | |
| DIST. ADM. | | DATE | BY | REVISION | | | |



N-145th. Sta. 143+00 to Sta. 143+41.57

⬡ Compacted Depth
① Asphalt Concrete Pavement Class B



N-145th. Sta. 143+41.57 to Sta. 148+61.57

See Sheet 24 For Roadway Section And Detours For Br. 513/32.

FOR "AS CONSTRUCTED PLANS" ONLY

| | | | | | | | | | | | | | | |
|-------------|--|-------------------|--|-------------------|------------------------------------|-----------|--------------|------------------|--|---|--|-------------------------------------|--|----------------------|
| DRAWN | | REGION NO. 10 | | STATE WASH | FED. AID PROJ. NO. IR-IRG 5-3(622) | SHEET NO. | TOTAL SHEETS | HIGHWAY DIVISION | | Washington State Department of Transportation | | SR 5 NE 145TH STREET INTERCHANGE | | SHEET 3 OF 24 SHEETS |
| CHECKED | | JOB NUMBER 89W041 | | CONTRACT NO. 3643 | | | | | | | | Roadway Sections - 5th Ave NE. | | |
| PROJ. ENGR. | | DATE | | REVISION | | BY APP'D | | | | | | | | |

[illegible]

GENERAL NOTES

1. APPROXIMATE NUMBER OF ITEMS TO BE REMOVED
2. EXISTING TRAFFIC BUTTONS TYPE A
3. EXISTING LANE MARKERS

SEE GENERAL NOTES

**FOR "AS CONSTRUCTED
PLANS" ONLY**

BRUNING 116378

TOTALS ONLY CHANGED

QUANTITY TABULATION - TRAFFIC AND OTHER ITEMS

NOTE:
THE SHEET NUMBER REFERS
TO THE PLAN SHEET SHOWING
THE CONSTRUCTION FEATURE.

GENERAL NOTES

| SH. NO. | LOCATION | CEMENT CONCRETE CURB & GUTTER L.F. | PAINT STRIPE L.F. | PAINTED CURB STRIPE L.F. | PAINTED CROSSWALK STRIPE L.F. | PLASTIC CROSSWALK STRIPE L.F. | PLASTIC STOP BAR L.F. | PLASTIC TRAFFIC ARROW EACH | PLASTIC TRAFFIC LETTER EACH | RAISED PAVEMENT MARKER TYPE 1 HUND. | RAISED PAVEMENT MARKER TYPE 2 HUND. | CEMENT CONCRETE SIDEWALK S.F. | CURB BUMP EACH | | | | | | | SEE GENERAL NOTES |
|---------|--|--|----------------------|-----------------------------|-------------------------------------|-------------------------------------|--------------------------|----------------------------------|-----------------------------------|---|---|-------------------------------------|-------------------|--|--|--|--|--|--|-------------------|
| | 9+150 TO 0+12 LT & RT | | 198 | | | | | | | | | | | | | | | | | |
| | 5+64 TO 6+00 (52°R TO 22°R) | 53 | | | | | | | | | | 40.4 | | | | | | | | |
| | NE 145TH 9+400 (42°RT) | | | | | | 16 | | | | | | | | | | | | | |
| | NE 145TH 9+750 (42°RT) | | | | | 50 | | | | | | | | | | | | | | |
| | NE 145th 9+450(30°L) TO S-145th 150+31(9°L) | 40 | | | | | | | | | | 27.0 | | | | | | | | |
| | 9+680 TO 0+24 (34°LT) | | | | | 55 | | | | | | | | | | | | | | |
| | 9+700 TO 0+23 (41°LT) | | | | | 47 | | | | | | | | | | | | | | |
| | 9+930 (37°RT) | | | | | | | | | | | | 1 | | | | | | | |
| | 0+05 (5°RT TO 32°RT) | | | | | | 27 | | | | | | | | | | | | | |
| | S-145th 150+14 TO 151+14(15°R) | | | 100 | | | | | | | | | | | | | | | | |
| | S-145th 150+68 (LT & RT) | | | | | | | 2 | | | | | | | | | | | | |
| | 0+10 (37°RT) | | | | | | | | | | | | 1 | | | | | | | |
| | 0+12(41°RT) TO 0+34(41°RT) | | | | | 21 | | | | | | | | | | | | | | |
| | 0+14(34°RT) TO 0+30(34°RT) | | | | | 25 | | | | | | | | | | | | | | |
| | 0+41(30°LT) TO 0+48(6°LT) | | | | | | 25 | | | | | | | | | | | | | |
| | 0+48(17°LT) TO 4+10(17°LT) | | | | | | | | | 0.45 | 0.06 | | | | | | | | | |
| | 0+40 TO 2+36 (6°LT) | | | 188 | | | | | | | | 0.10 | | | | | | | | |
| | 0+40(17°RT) TO 3+00(17°RT) | | | | | | | | | 0.45 | 0.06 | | | | | | | | | |
| | 0+73 (6°LT TO 6°RT) | | | | | | 12 | | | | | | | | | | | | | |
| | 0+73 TO 4+72 (VARIES) | | 526 | | | | | | | | 0.52 | | | | | | | | | |
| | 1+01 | | | | | | | 1 | | | | | | | | | | | | |
| | 1+16 | | | | | | | | 4 | | | | | | | | | | | |
| | 2+25 | | | | | | | 1 | | | | | | | | | | | | |
| | 4+72 (6°LT TO 29°RT) | | | | | | 35 | | | | | | | | | | | | | |
| | SHEET TOTAL | 95 | 724 | 288 | 0 | 198 | 115 | 4 | 4 | 0.90 | 0.74 | 67.4 | 2 | | | | | | | |

FOR "AS CONSTRUCTED
PLANS" ONLY

| | | | | | | | | | | | | | | | | | | | |
|----------------|-------|---------|------------|----------|------|------|----------|----|---------------|-------------|------------------------------------|-------------------|-------------------|------------------|----------|---|----------------------------------|---------------------|---------------|
| BRUNING 116376 | DRAWN | CHECKED | PROJ. ENGR | DIS. ADM | DATE | DATE | REVISION | BY | REGION NO. 10 | STATE WASH. | FED. AID PROJ. NO. IR-IRG-5-3(622) | JOB NUMBER B9W041 | CONTRACT NO. 3643 | HIGHWAY DIVISION | APPROVED | Washington State Department of Transportation | SR 5 NE 145TH STREET INTERCHANGE | QUANTITY TABULATION | SHEET 5 OF 24 |
|----------------|-------|---------|------------|----------|------|------|----------|----|---------------|-------------|------------------------------------|-------------------|-------------------|------------------|----------|---|----------------------------------|---------------------|---------------|

DOT FORM 227-81
R/12/87

SR 5/2559

TOTALS ONLY CHANGED

QUANTITY TABULATION - TRAFFIC AND OTHER ITEMS

NOTE:
THE SHEET NUMBER REFERS
TO THE PLAN SHEET SHOWING
THE CONSTRUCTION FEATURE.

GENERAL NOTES

| SH. NO. | LOCATION | CEMENT CONCRETE CURB & GUTTER | PAINT STRIPE | PAINTED CURB STRIPE | PAINTED CROSSWALK STRIPE | PLASTIC CROSSWALK STRIPE | PLASTIC STOP BAR | PLASTIC TRAFFIC ARROW | PL/STIC TRAFFIC LETTER | RAISED PAVEMENT MARKER TYPE 1 | RAISED PAVEMENT MARKER TYPE 2 | CEMENT CONCRETE SIDEWALK | CURB RAMP | | | | | | SEE GENERAL NOTES |
|---------|-----------------------------|----------------------------------|--------------|---------------------|-----------------------------|-----------------------------|------------------|--------------------------|---------------------------|----------------------------------|----------------------------------|-----------------------------|-----------|--|--|--|--|--|-------------------|
| | | L.F. | L.F. | L.F. | L.F. | L.F. | L.F. | EACH | EACH | HUND | HUND | S.F. | EACH | | | | | | |
| | 4+78(51*LT) TO 5+70(39*LT) | | | | | 94 | | | | | | | | | | | | | |
| | 4+85(44*RT) TO 5+72(40*RT) | | | | 82 | | | | | | | | | | | | | | |
| | 4+85(54*RT) TO 5+00(59*RT) | | | | | | | | | | | 10.00 | | | | | | | |
| | 4+86(57*LT) TO 5+67(47*LT) | | | | | 83 | | | | | | | | | | | | | |
| | 4+95(50*RT) TO 5+71(47*RT) | | | | 70 | | | | | | | | | | | | | | |
| | 5+69(52*LT) TO 5+79(39*LT) | | | | | | | | | | | 14.60 | | | | | | | |
| | 5+70(38*LT) TO 5+72(40*RT) | | | | 71 | | | | | | | | | | | | | | |
| | 5+77(30*LT) TO 5+79(30*RT) | | | | 57 | | | | | | | | | | | | | | |
| | N-145TH 143+00 TO 148+45 | | 745 | | | | | | | | | | | | | | | | |
| | N-145TH 143+61.57 TO 148+45 | | | 260 | | | | | | 1.51 | | | | | | | | | |
| | N-145TH 144+00 | | | | | | 14 | | | | | | | | | | | | |
| | N-145TH 144+95 LT | | | | | | 1 | | | | | | | | | | | | |
| | N-145TH 147+05 LT AND RT | | | | | | | | 8 | | | | | | | | | | |
| | N-145TH 147+15 LT AND RT | | | | | | | 3 | | | | | | | | | | | |
| | N-145TH 147+95 LT AND RT | | | | | | | 3 | | | | | | | | | | | |
| | SHEET TOTAL | 0 | 745 | 260 | 280 | 177 | 14 | 6 | 8 | 1.51 | 0.00 | 24.80 | 0 | | | | | | |
| | PROJECT TOTAL | 108 | 2563 | 100 | 0 | 611 | 151 | 11 | 12 | 4.36 | 0.31 | 50.5 | 1 | | | | | | |

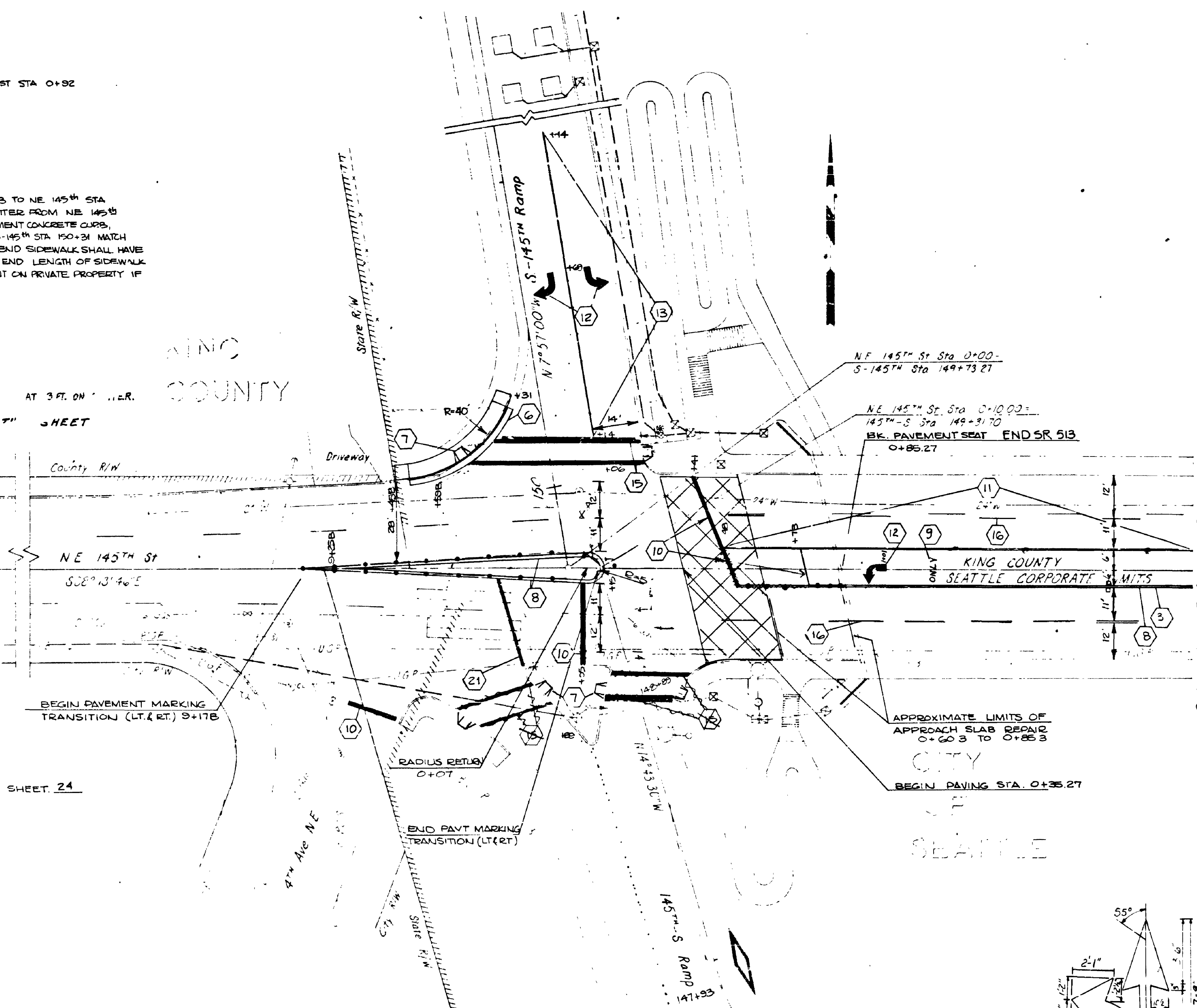
FOR "AS CONSTRUCTED
PLANS" ONLY

| | | | | | | | | | | | | | | |
|-----------------------------|------------------------|------------|------|------|----------|----|--------------|-------|--------------------|------------------|--|-------------------------------------|---------------------|-----------------------------------|
| DRAWN | CHECKED PROJ. ENGR. | DIST. ADM. | DATE | DATE | REVISION | BY | REGION | STATE | FED. AID PROJ. NO. | HIGHWAY DIVISION | Washington State Department of Transportation | SR 5 NE 145TH STREET INTERCHANGE | QUANTITY TABULATION | SHEET 6 OF 24 5/25/87 |
| | | | | | | | IC | WASH. | IR-1RG-5-3(622) | | | | | |
| | | | | | | | JOB NUMBER | W041 | | | | | | |
| | | | | | | | CONTRACT NO. | 3693 | | | | | | |
| DOT FORM 221-081 R/12/87 | | | | | | | | | | APPROVED | | | | |

SR 5/2557

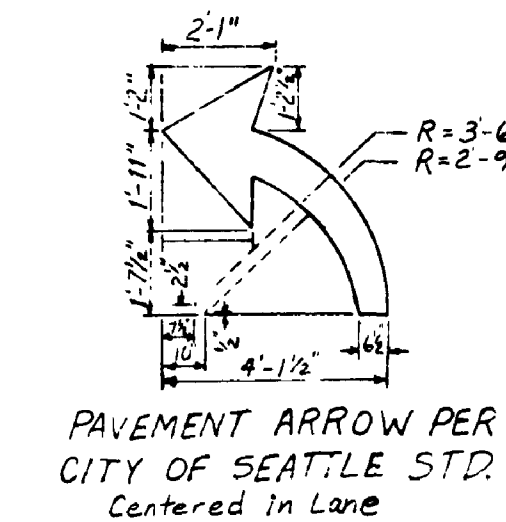
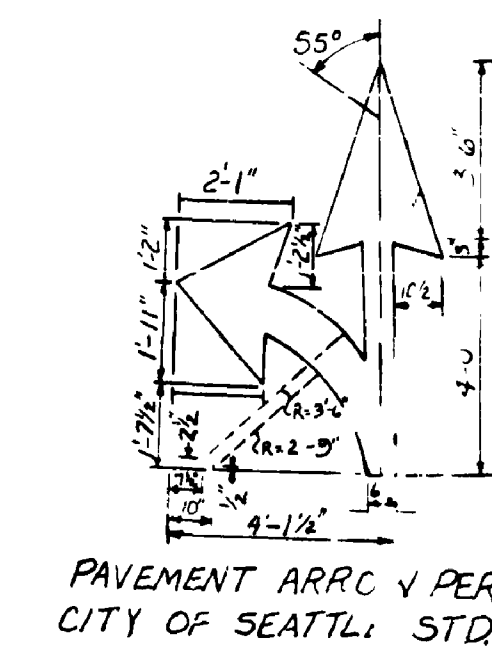
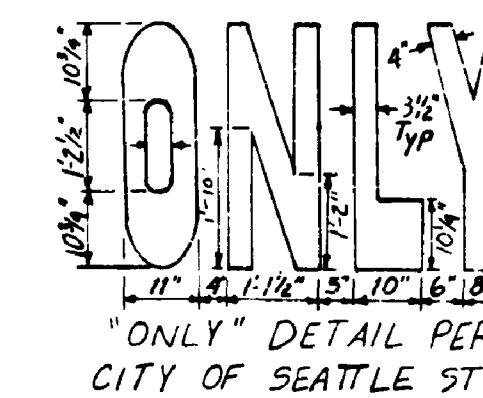
- 1 NOT USED
- 2 NOT USED
- 3 REMOVE TYPE C TRAFFIC CURB FROM NE 145th ST STA 0+02 TO NE 145th STA 2+34
- 4 NOT USED
- 5 NOT USED
- 6 REMOVE EXISTING CURB FROM NE 145th STA 9+45B TO NE 145th STA 9+59B. REMOVE EXISTING ROLLED CONCRETE GUTTER FROM NE 145th STA 9+59B TO S-145th STA 150+31. CONSTRUCT CONCRETE CURB, GUTTER AND SIDEWALK FROM NE 145th STA 9+45B TO S-145th STA 150+31. MATCH CURB AND GUTTER TO EXISTING CURB OR GUTTER AT EACH END. SIDEWALK SHALL HAVE A 12:1 SLOPE TO MEET THE EXISTING SHOULDER AT EACH END. LENGTH OF SIDEWALK AND CURB SHALL BE ADJUSTED TO AVOID ENCROACHMENT ON PRIVATE PROPERTY IF NECESSARY.
- 7 INSTALL CURB RAMP
- 8 INSTALL 4" DOUBLE YELLOW CENTER STRIPE.
- 9 INSTALL PLASTIC TRAFFIC LETTERS "ONLY" SEE DETAIL THIS SHEET.
- 10 INSTALL PLASTIC STOP BAR.
- 11 INSTALL 4" Painted GORE STRIPE AND TYPE 1 R.P.M.* AT 3 FT. ON CENTER.
- 12 INSTALL PLASTIC TRAFFIC ARROW SEE DETAIL 7" SHEET
- 13 INSTALL 8" Painted GORE STRIPE BETWEEN STA 150+14 AND S-145th STA 150+31.
- 14 INSTALL CROSSWALK (2.0#4)
- 15 R.P.M.* AS SHOWN IN STANDARD PLAN H-3 BETWEEN NE 145th STA 0+48 AND NE 145th STA 3+80
- 16 REMOVE ASPHALT AND C-CURB TRAFFIC ISLAND.
- 17 4" WHITE SKIP STRIPE WITH TYPE 1 LANE MARKERS AT 2.5 FT. ON CENTER.
- 18 NOT USED
- 19 NOT USED
- 20 NOT USED
- 21 REMOVE EXISTING PAINTED STOP BAR.
- 22 INSTALL PAVEMENT TRANSITION. SEE DETAIL SHEET 24

R.P.M.* = RAISED PAVEMENT MARKERS



LEGEND

- UGP Underground Power
- W Waterline
- SM Sewer Manhole
- F Fence
- TJ Type I Junction Box
- V Valve
- C Controller
- L Luminaire
- NG Natural Gas Line
- U Utility Pole
- EL Existing Light Fixture
- CR Curb Ramp
- PS Pedestrian Signal Pole
- CB Catch Basin
- WM Water Manhole
- PN Pacific Northwest Bell-Engineering Co.
- FH Fire Hydrant
- SW Wire and Anchor
- TJ Type II Junction Box
- CR Curb Ramp
- EX Existing ACP to be planed
- CS Concrete Spot Post

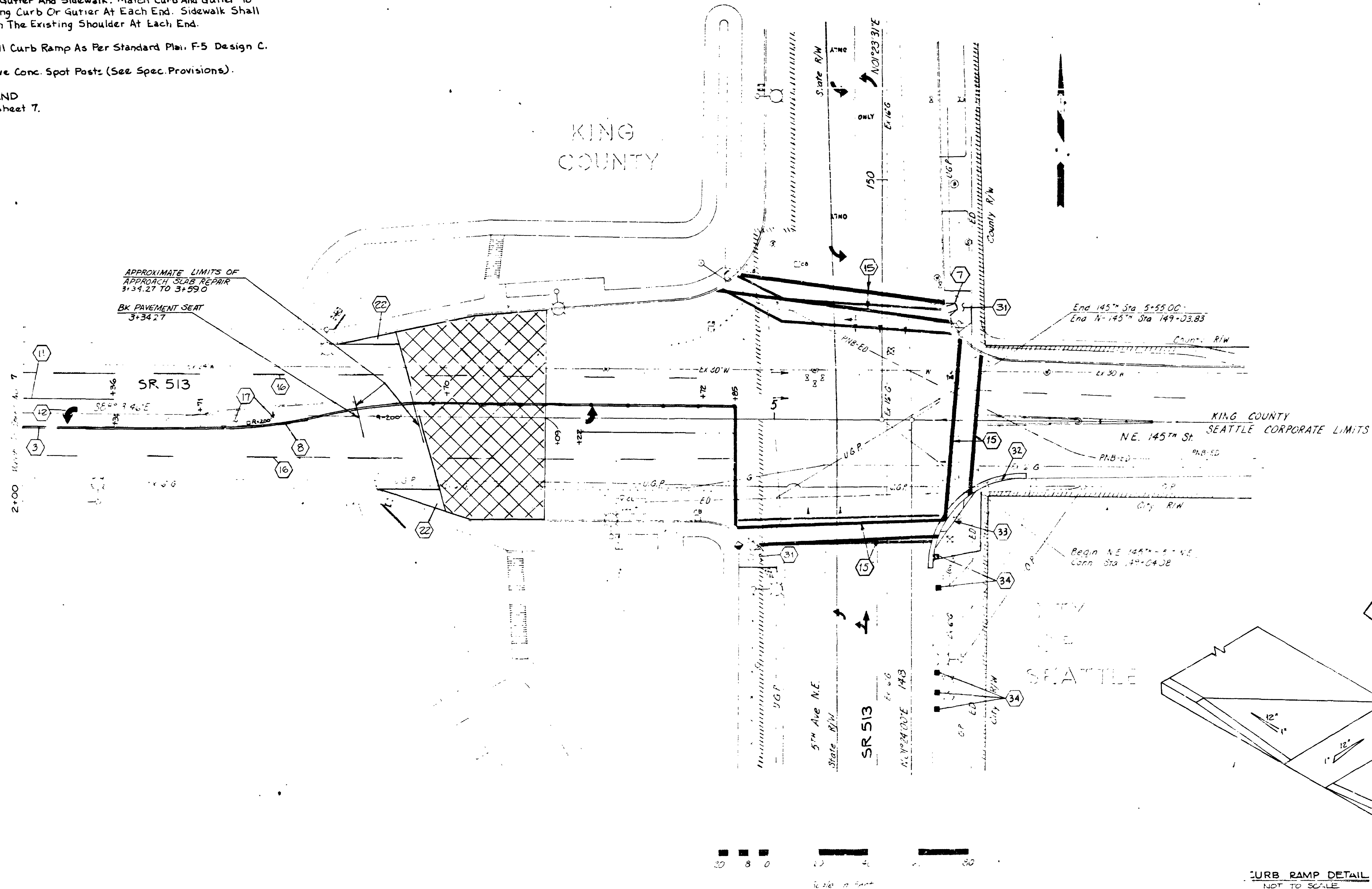


FOR "AS CONSTRUCTED PLANS" ONLY

| | | | | | | | | | | | | |
|----------------------------|----------|-------|------|---------|--------------------|-----------|--------------|----------------------|---|------|----------------------------------|----------------------|
| ENGINEER DAN TAMSKY | | | | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS | DIVISION OF HIGHWAYS | Washington State Department of Transportation | SR 5 | NE 145TH STREET INTERCHANGE | SHEET 7 OF 24 SHEETS |
| DRAWN JIM OLSEN | | | | 10 WASH | IR-IRG-5-3(622) | | | | | | CHANNELIZATION AND PAVING - WEST | |
| REVIEWER JOHN KERR | | | | | | | | | | | | |
| CHECKED HARRY ROLLINS | | | | | | | | | | | | |
| PROJ. ENGR. CLIFF KURTZWEG | | | | | | | | | | | | |
| DIST. ADM. RON BORKSTRAK | | | | | | | | | | | | |
| DATE | REVISION | APP'D | 3643 | | | | | APPROVED | | | | |

- (31) Install Cement Concrete Sidewalks Where Shown.
Match Edges To Existing Sidewalk And Curb.
- (32) Remove Existing Curb From N-145th Sta. 148+43 Rt. To 145th Sta. 6+10 Rt. Construct 35' Radius Cement Concrete Curb, Gutter And Sidewalk. Match Curb And Gutter To Existing Curb Or Gutter At Each End. Sidewalk Shall Match The Existing Shoulder At Each End.
- (33) Install Curb Ramp As Per Standard Plan, F-5 Design C.
- (34) Remove Conc. Spot Posts (See Spec. Provisions).

LEGEND
See Sheet 7.



| ENGINEER | DAN TAMSKY | STATE | 10 | FED. AID PROJ. NO. | IR-5 3(622) | SHEET NO. | TOTAL SHEETS |
|-------------|----------------|-------|-------|--------------------|-------------|-----------|--------------|
| DRAWN | JIM OLSEN | | | | | | |
| REVIEWER | JOHN KERR | | | | | | |
| CHECKED | HARRY ROWNS | | | | | | |
| PROJ. ENGR. | CLIFF KURTZWEG | | | | | | |
| DIST. ADM. | RON BOCKSTRUCK | | | | | | |
| DATE | REVISION | BY | APP'D | 3643 | | | |

DIVISION OF HIGHWAYS

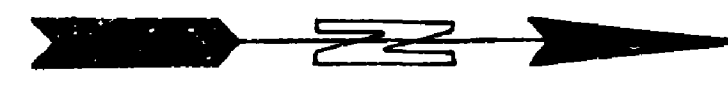
APPROVED



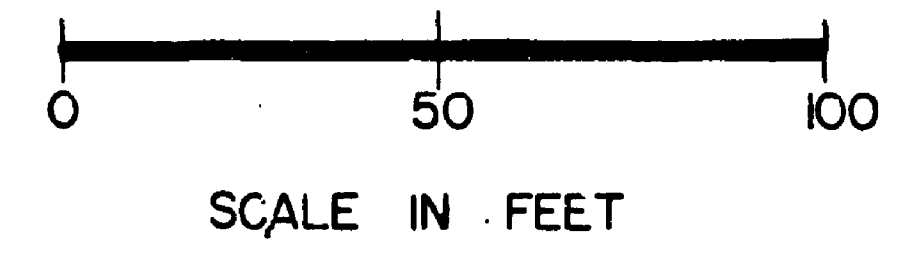
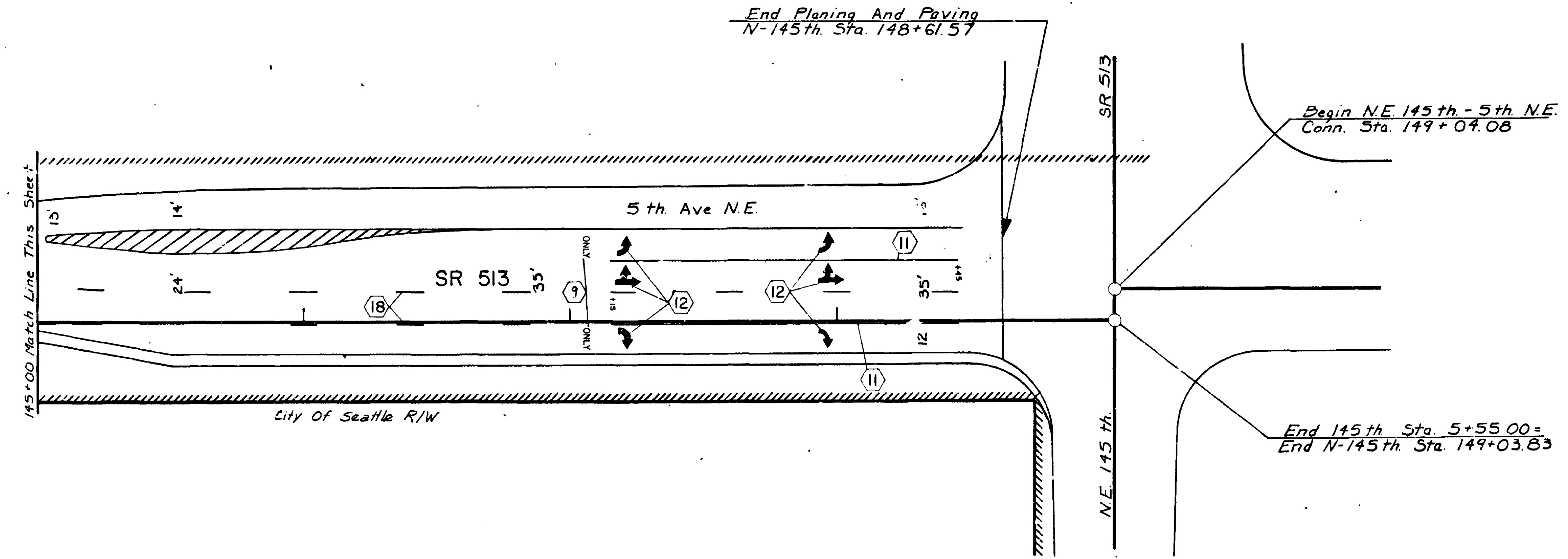
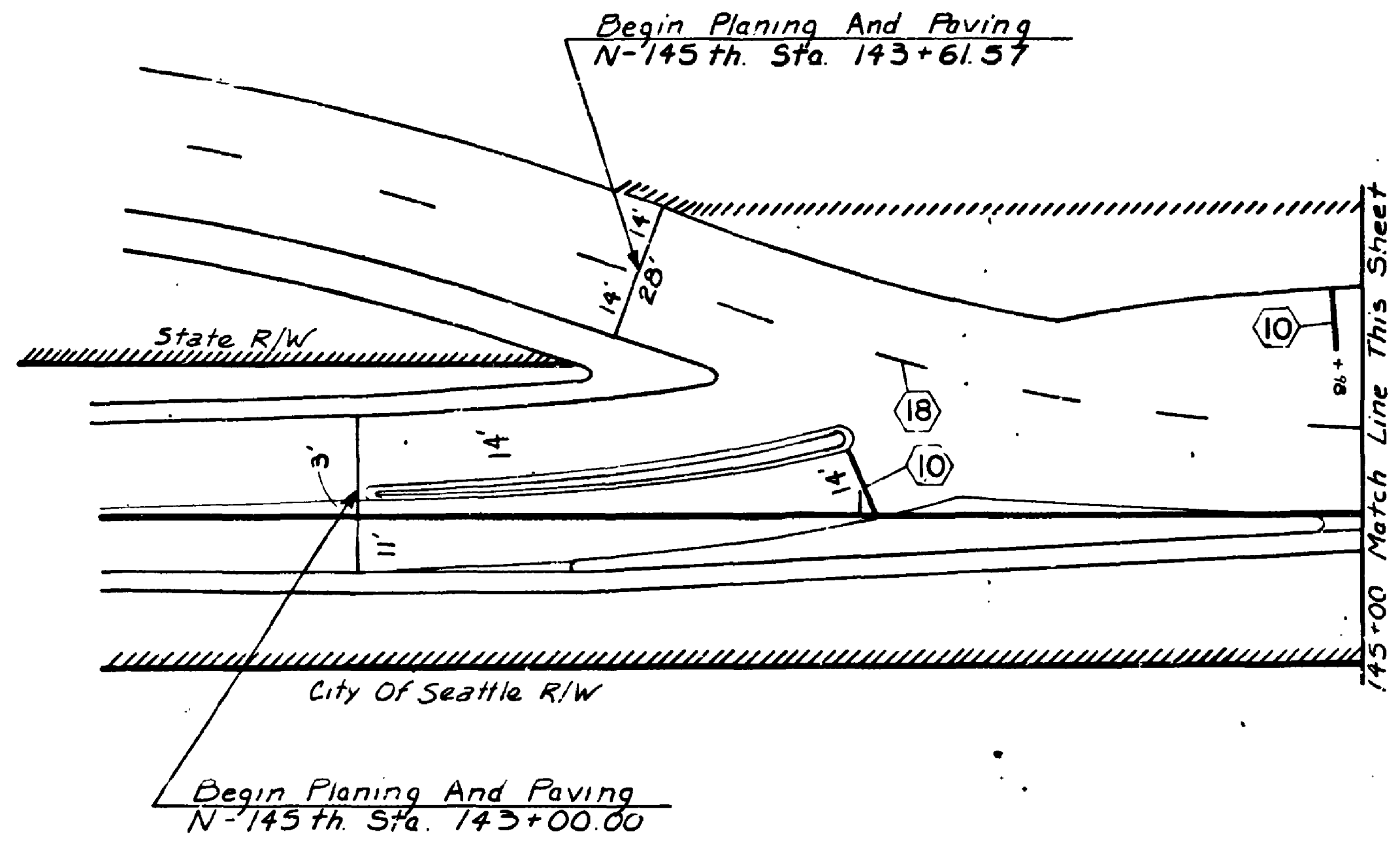
Washington State
Department of Transportation

SR 5
NE 145TH STREET INTERCHANGE
CHANNELIZATION AND PAVING-
EAST


SHEET
8
OF
24
SHEETS



General Notes:
1. Planing Depth From N-145th
Sta. 143+00 to 148+61.57 Shall be 0.15'
2. Minimum Compacted Depth of A.C.P. C.I. B
From N-145th, Sta. 143+00 to 148+61.57
Shall Be 0.15'.
Note: For Construction Notes See Sheet 7.



FOR "AS CONSTRUCTED
PLANS" ONLY

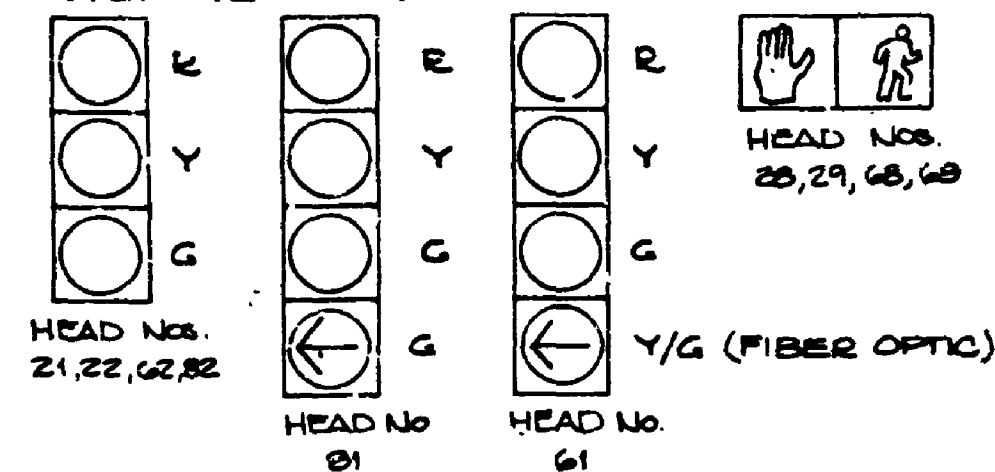
| | | | | | | | | | |
|---------------------------------|------------|----------|----------------------|-----------|--------------|------------------|--|--|----------------------------------|
| DESIGNED <i>D. McCormick</i> | REGION NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS | HIGHWAY DIVISION |  Washington State Department of Transportation | SR 5 NE 145TH STREET INTERCHANGE | SHEET 9 OF 24 SHEETS |
| DRAWN <i>D. McCormick</i> | 10 | WASH. | IR 5-3(622) | | | | | | |
| CHECKED | | | JOB NUMBER 89W041 | | | | | | |
| PROJ. ENGR. <i>N.N. Storme</i> | | | CONTRACT NO. 3643 | | | | | | |
| DIST. ADM. <i>RE Bockstruck</i> | DATE | REVISION | BY | APP'T. | | | | CHANNELIZATION AND PAVING 5th AVE. N.E. | |

CONSTRUCTION NOTES FOR SHEETS 12 AND 13

1. INSTALL TYPE II VEHICLE DETECTION LOOP. SEE STANDARD PLAN J-8a.
2. INSTALL 50' TYPE I VEHICLE DETECTION LOOP. SEE STANDARD PLAN J-8a.
3. INSTALL 30' TYPE I VEHICLE DETECTION LOOP. SEE STANDARD PLAN J-8a.
4. INSTALL 19' MODIFIED TYPE 1 VEHICLE DETECTION LOOP. SEE STANDARD PLAN J-8a. SEE DETAIL SHEET 14.
5. INSTALL 20' MODIFIED TYPE 1 VEHICLE DETECTION LOOP. SEE STANDARD PLAN J-8a. SEE DETAIL SHEET 14.
6. INSTALL 15' MODIFIED TYPE 1 VEHICLE DETECTION LOOP. SEE STANDARD PLAN J-8a. SEE DETAIL SHEET 14.
7. INSTALL TWO VEHICLE SIGNAL HEADS, ONE PEDESTRIAN SIGNAL HEAD, LUMINAIRE, ONE TYPE 1-AB PEDESTRIAN PUSH BUTTON AND TERMINAL CABINET ON POLE. SEE STANDARD PLAN J-5a AND SIGNAL STANDARD DETAIL SHEET.
8. INSTALL ONE PEDESTRIAN SIGNAL HEAD AND ONE TYPE 1-AB PEDESTRIAN PUSH BUTTON ON Type 1 Pole.
9. INSTALL FOUR VEHICLE SIGNAL HEADS, ONE PEDESTRIAN SIGNAL HEAD, LUMINAIRE, ONE TYPE 1-AB PEDESTRIAN PUSH BUTTON AND TERMINAL CABINET ON POLE. SEE STANDARD PLAN J-5a, 2 AND (DIE) SIGNAL STANDARD DETAIL SHEET.
10. INSTALL CONTROLLER CABINET BASE PER STANDARD PLAN J-6c. INSTALL CONTROLLER CABINET, CONTROLLER, 120/240 VOLT MODIFIED TYPE "D" SERVICE AND CABINET (SUA 1124), AND ASSOCIATED EQUIPMENT. CONTROLLER CABINET DOOR SHALL FACE EAST WITH HINGE ON LEFT SIDE. BOTH CABINETS SHALL BE LOCATED SO THAT THE DOORS CAN BE OPENED THROUGH 180° WITHOUT STRIKING ANY FIXED OBJECT. SEE SPECIAL PROVISIONS AND ELECTRICAL SERVICE DETAILS.
11. Seal Cable Ends
12. INSTALL MODIFIED TYPE 2 DETECTION LOOP. SEE DETAIL SHEET 14.
13. CONTINUE RUN 14 UP THE POWER POLE USING CONDUIT RISER AND WEATHER HEAD PER THE UTILITY COMPANY'S REQUIREMENTS. COORDINATE WORK WITH THE UTILITY COMPANY TO CONNECT NEW SERVICE CABLES AND DISCONNECT EXISTING SERVICE CABLES (EXISTING TYPE A SERVICE SUA 1124).

T.26N. R.4E. W.M.

SIGNAL HEAD ASSIGNMENTS - WEST



SIGNAL HEAD ASSIGNMENT NOTES - WEST

1. VEHICLE SIGNAL HEADS SHALL BE MOUNTED USING MOUNTING TYPE "M" WITH THE ELEVATOR PLUMBIZER LOCATED BETWEEN THE RED AND YELLOW SECTIONS. SEE STANDARD PLAN J-6g.
2. VEHICLE SIGNAL HEADS SHALL HAVE TUNNEL VISORS, EXCEPT SIGNAL HEAD #22 SHALL HAVE CAN VISORS.
3. ALL PEDESTRIAN SIGNAL HEADS SHALL USE THE INTERNATIONAL HAND-MAN SYMBOL, BE THE NEON GRID TYPE, AND SHALL HAVE 2 CRATE VISORS.
4. All Vehicle Signal Heads Shall Have 12" Lenses.
5. Pedestrian Signal Head Mounting Shall Be: No. 29, 69 Mounting Type D And No. 28, 68 Mounting Type B.

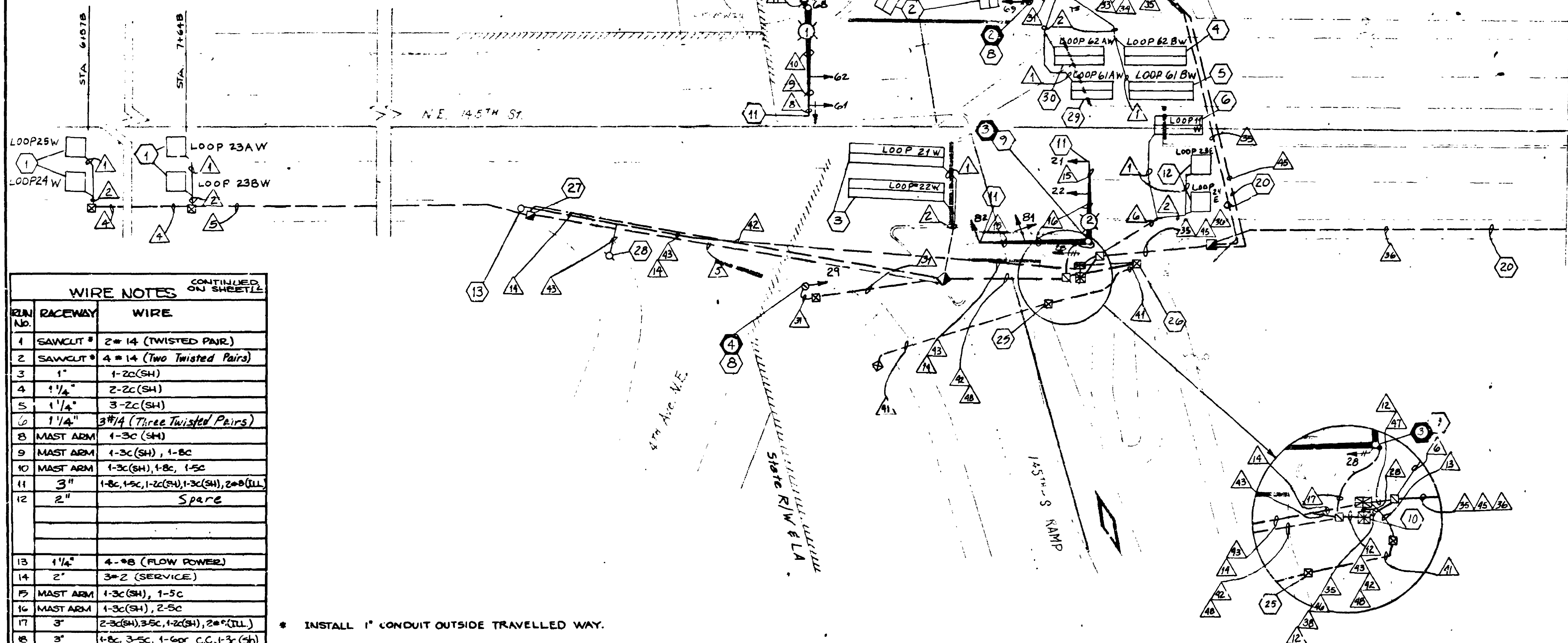
LEGEND

- Controller (Existing)
- Luminaire
- Utility Pole
- Pedestrian Signal Pole
- Type 1 Junction Box
- Type 2 Junction Box
- Type 3 Junction Box
- Curb Ramp
- Conduit (See Table Below)
- Wire Note / Run No.
- Construction Note
- Pole Identification Note
- Signal Pole Type II With Terminal Cabinet

LABEL CONDUIT SYSTEM

- Signal Pole Type III With Terminal Cabinet
- Vehicle Signal Head
- Vehicle Signal Head With Arrow Indication
- Pedestrian Signal Head
- Controller Cabinet
- Modified Type "D" Service

For Luminaire Schedule, See Sheet 12.



FOR "AS CONSTRUCTED PLANS" ONLY

| WIRE NOTES | | |
|------------|----------|--|
| WIRE NO. | RACEWAY | WIRE |
| 1 | SAW CUT | 2# 14 (TWISTED PAIR) |
| 2 | SAW CUT | 4# 14 (Two Twisted Pairs) |
| 3 | 1" | 1-2c(SH) |
| 4 | 1 1/4" | 2-2c(SH) |
| 5 | 1 1/4" | 3-2c(SH) |
| 6 | 1 1/4" | 3# 14 (Three Twisted Pairs) |
| 7 | MAST ARM | 1-3c(SH) |
| 8 | MAST ARM | 1-3c(SH), 1-5c |
| 9 | MAST ARM | 1-3c(SH), 1-5c |
| 10 | MAST ARM | 1-3c(SH), 1-5c |
| 11 | 3" | 1-8c, 1-5c, 1-2c(SH), 1-3c(SH), 2# 14(ILL) |
| 12 | 2" | Spare |
| 13 | 1 1/4" | 4# 8 (FLOW POWER) |
| 14 | 2" | 3# 2 (SERVICE) |
| 15 | MAST ARM | 1-3c(SH), 1-5c |
| 16 | MAST ARM | 1-3c(SH), 2-5c |
| 17 | 3" | 2-3c(SH), 3-5c, 1-2c(SH), 2# 14(ILL) |
| 18 | 3" | 1-8c, 3-5c, 1-4pr C.C., 1-3c(SH) |
| 19 | 3" | 2-3c(SH), 1-6-2c(SH) |
| 20 | 2-2" | SPARE, 2-5c |
| 21 | MAST ARM | 1-3c(SH), 3-5c |
| 22 | 2 1/2" | 1-8c, 1-5c, 1-3c(SH), 2-2c(SH) |
| 23 | 3" | 1-8c, 1-5c, 1-3c(SH), 1-2c(SH) |
| 24 | 2 1/2" | 2-8c, 1-3c(SH), 2-2c(SH) |
| 25 | 2" | 6-2c(SH) |
| 26 | 2" | 2-5c, 1-2c(SH), 1-2c(SH) |
| 27 | 2" | 8-2c(SH) |

* INSTALL 1" CONDUIT OUTSIDE TRAVELLED WAY.

CC - COMMUNICATION CABLE
SH - SHIELDED CABLE
ILL - ILLUMINATION

| ENGINEER | DRAWN | CHECKED | PROJ. ENGR. | DIST. ADM. | DATE | REVISION | BY | APP'D | CONTRACT NO. | 3643 |
|--------------|-----------|-----------|--------------|----------------|----------------|----------|----|-------|--------------|------|
| DANIEL TMSKY | JIM OLSEN | JOHN KERR | HARRY ROWINS | CLIFF KURTZWEG | RON BOCKSTRUCK | | | | | |

DIVISION OF HIGHWAYS

APPROVED

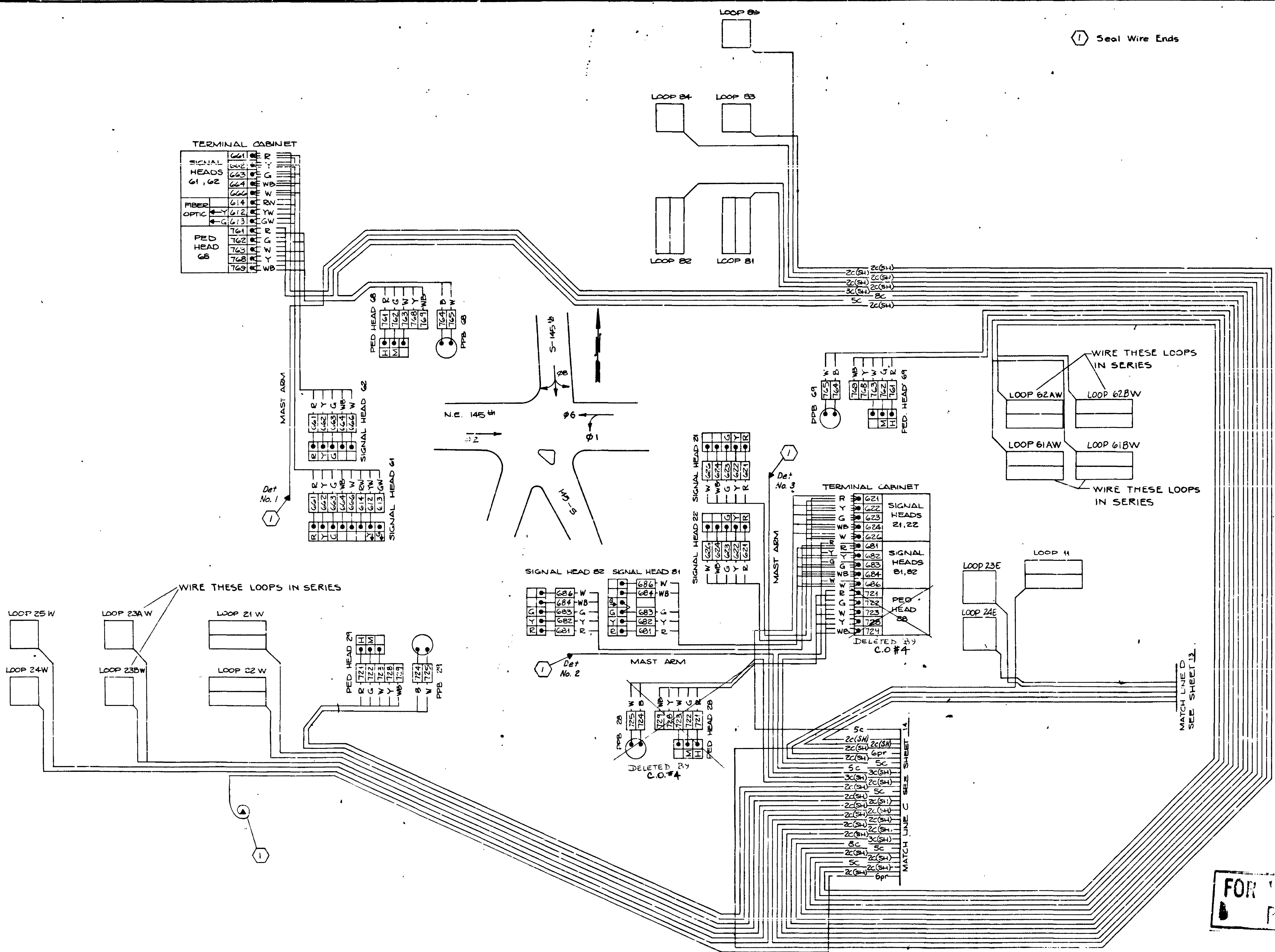
Washington State
Department of Transportation

SR 5
NE 145TH STREET INTERCHANGE

SIGNAL PLAN - WEST

SHEET
10
OF
24
SHEETS

Seal Wire Ends



FOR INSTRUCTIONS
ONLY

| | | | | | | | | | | | | | | | | | | | | | |
|---------------------------|--|---------------------------|--|------------------------------------|--|--------------|--|-----------------|--|----------------------|--|---|--|------|--|-----------------------------|--|-------------------------|--|----------------|--|
| ENGINEER DANIEL TAMSKY | | STATE 10 | | FED. AID PROJ. NO. IR-IRG-5-3(622) | | SHEET NO. 11 | | TOTAL SHEETS 24 | | DIVISION OF HIGHWAYS | | Washington State Department of Transportation | | SR 5 | | NE 145TH STREET INTERCHANGE | | WIRING SCHEMATIC - WEST | | SHEET 11 OF 24 | |
| DRAWN JIM OLSEN | | FED. AID PROJ. NO. 89W041 | | CONTRACT NO. 3643 | | APPROVED | | DATE | | REVISION | | BY AP. D | | DATE | | REVISION | | BY AP. D | | DATE | |
| REVIEWER JOHN KERR | | PROJECT NO. 89W041 | | CONTRACT NO. 3643 | | APPROVED | | DATE | | REVISION | | BY AP. D | | DATE | | REVISION | | BY AP. D | | DATE | |
| CHECKED HARRY ROWINS | | PROJECT NO. 89W041 | | CONTRACT NO. 3643 | | APPROVED | | DATE | | REVISION | | BY AP. D | | DATE | | REVISION | | BY AP. D | | DATE | |
| PROJ. ENGR. CLIFF WITZWEG | | PROJECT NO. 89W041 | | CONTRACT NO. 3643 | | APPROVED | | DATE | | REVISION | | BY AP. D | | DATE | | REVISION | | BY AP. D | | DATE | |
| DIST. ADM. RON BOCKSTUCK | | PROJECT NO. 89W041 | | CONTRACT NO. 3643 | | APPROVED | | DATE | | REVISION | | BY AP. D | | DATE | | REVISION | | BY AP. D | | DATE | |

DOT FORM 221-011
Revised 6/80

CONSTRUCTION NOTES CONT'D

14. REPLACE EXISTING TYPE I JUNCTION BOX WITH NEW TYPE II JUNCTION BOX. THE EXISTING JUNCTION BOX WILL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL NOT BE USED ON THIS CONTRACT.
15. NOT USED
16. INSTALL TWO VEHICLE SIGNAL HEADS, ONE PEDESTRIAN SIGNAL HEAD, ONE TYPE I-AB PEDESTRIAN PUSH BUTTON AND TERMINAL CABINET ON POLE. SEE STANDARD PLAN J-50 AND SIGNAL STANDARD DETAIL SHEET.
17. INSTALL THREE VEHICLE SIGNAL HEADS, TWO PEDESTRIAN SIGNAL HEADS, TWO TYPE I-AB PEDESTRIAN PUSH BUTTONS AND TERMINAL CABINET ON POLE.
18. INSTALL TWO VEHICLE SIGNAL HEADS, TWO PEDESTRIAN SIGNAL HEADS, TWO TYPE I-AB PEDESTRIAN PUSH BUTTONS, AND TERMINAL CABINET ON POLE.
19. INSTALL CONTROLLER CABINET BASE PER STANDARD PLAN J-60. INSTALL CONTROLLER CABINET, CONTROLLER, AND ASSOCIATED EQUIPMENT. CONTROLLER CABINET DOOR SHALL FACE WEST WITH HINGE AT LEFT EDGE.
20. ATTACH CONDUIT TO BRIDGE WITH UNISTRUT AT 5' INTERVALS.
21. NOT USED
22. EXISTING CONDUIT RESERVED FOR FUTURE USE.
23. SPICE ILLUMINATION CONDUCTORS TO EXISTING 480V, SERVICE SUA 675, CIRCUIT NO. 6 IN EXISTING JUNCTION BOX.
24. CONDUIT TO SERVICE CABINET SUA 675, LOCATED APPROXIMATELY AT N-145th RAMP STA 143+30. SPICE CONDUCTORS TO 120V SUPPLY AT SERVICE CABINET.

25. ROUTE NEW CONDUIT AND CABLES INTO EXISTING JUNCTION BOX. SPICE 2 NEW #8 CABLES TO EXISTING 2#8 POWER CABLES FOR ELECTRONIC SURVEILLANCE CABINET ES 15.8. DO NOT SPICE 2#8 POWER CABLES FOR ELECTRONIC SURVEILLANCE CABINET ES 15.8 AT THIS JUNCTION BOX.
26. SPICE 2 NEW #8 TO EXISTING 2#8 POWER CABLES FOR ELECTRONIC SURVEILLANCE CABINET ES 15.8. COIL EXISTING 30(SH) SIGN CONTROL CABLE ENDS IN THIS JUNCTION BOX. SEAL CABLE ENDS WITH AN APPROVED HEAT SHRINK END CAP.
27. PROVIDE AND INSTALL NEW TYPE I JUNCTION BOX. PROVIDE EXTRA 35 FT. OF 6 pr. COMMUNICATIONS CABLE. COIL CABLE IN JUNCTION BOX. SEAL CABLE END WITH APPROVED HEAT SHRINK END CAP.
28. PROVIDE AND INSTALL CONDUIT ON THIS POLE FOR TELEPHONE CABLE. SEE SPECIAL PROVISIONS.
29. CONSTRUCT 15' TYPE I VEHICLE DETECTION LOOP. SPICE LOOPS 61A AND 61B TO THE SAME 20(SH) CABLE. SEE STANDARD PLAN J-80.
30. CONSTRUCT 16' TYPE I VEHICLE DETECTION LOOP. SPICE LOOPS 62A AND 62B TO THE SAME 20(SH) CABLE. SEE STANDARD PLAN J-80.
31. REMOVE EXISTING CABLE FROM CONDUIT AND J.B. ABANDON LOOPS.
32. INSTALL PUSH BUTTON PEDESTAL AND BASE WITH 480/120 TRANSFORMER RATED AT 7.5 KVA WITH CABINET (C.O.#3).

T.26N. R.4E. W.M.

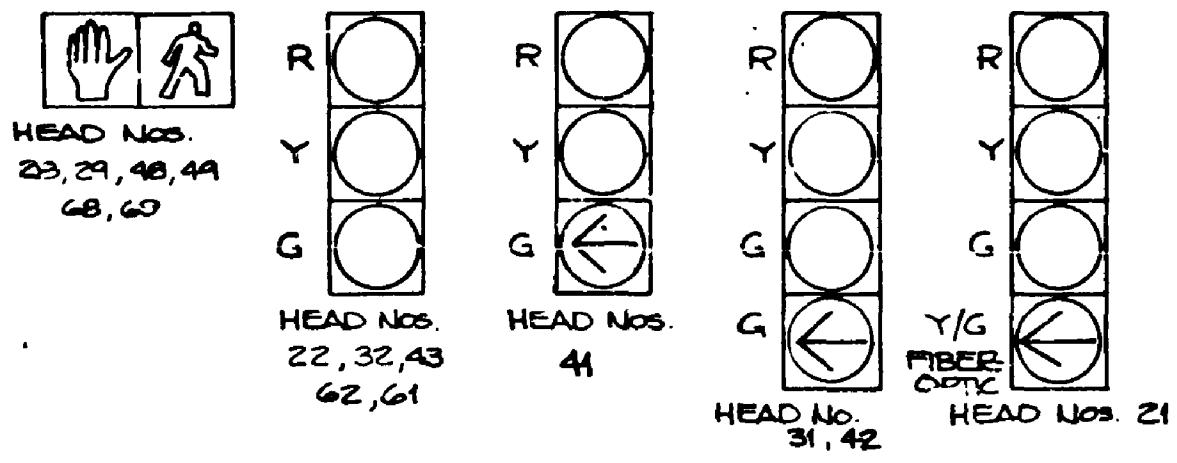
NOTE: For Wire Notes 1-23 See Sheet 10.

| WIRE NOTES | | |
|------------|---------|---|
| ROW No. | RACEWAY | WIRE |
| 26 | 1 1/2" | 5-2c(SH) |
| 27 | 2 1/2" | 2-5c, 1-3c(SH), 3-2c(SH) |
| 28 | 3" | 1-5c, 1-5c, 1-3c(SH), 7-2c(SH) |
| 29 | 1" | 2#8 (POWER) |
| 30 | 3" | 2-8c, 1-3c(SH), 0-2c(SH) |
| 31 | 1 1/4" | 1-6pr. CC |
| 32 | 2" | 1-2c(SH), 1-5c |
| 33 | 3" | 6-2c(SH), 1-5c, 1-6pr. CC |
| 34 | 3" | 1-5c, 1-5c, 6-2c(SH), 1-3c(SH), 2#8(SH) |
| 35 | 3" | 2-5c, 0-2c(SH), 1-3c(SH) |
| 36 | 3" | 6# CC 4-2c(SH) |
| 37 | | NOT USED |
| 38 | 2" | 1-6pr. CC, 3-2c(SH) |
| 39 | 3" | 1-5c, 2-5c, 9-2c(SH), 1-3c(SH) |
| 40 | 3" | 6-2c(SH), 1-5c, 1-6pr. CC |
| 41 | 1 1/2" | 2#8 (ILL.) |
| 42 | 1 1/2" | 2#8 (NEW FLOW), 1-3c(SH) + 1-6pr. CC |
| 43 | 1 1/4" | FULL ROPE FOR TELEPHONE CABLE |
| 44 | 3" | 2-8c, 1-3c(SH), 0-2c(SH) |
| 45 | 1" | 2#8 Power |
| 46 | 3" | 1-8c, 2#8 (ILL.) |
| 47 | 1 1/2" | 1-8c |
| 48 | 1" | 2#8 (ILL.) |
| | | 1-5c, 4-2c(SH) |

APPENDIX NO. 1

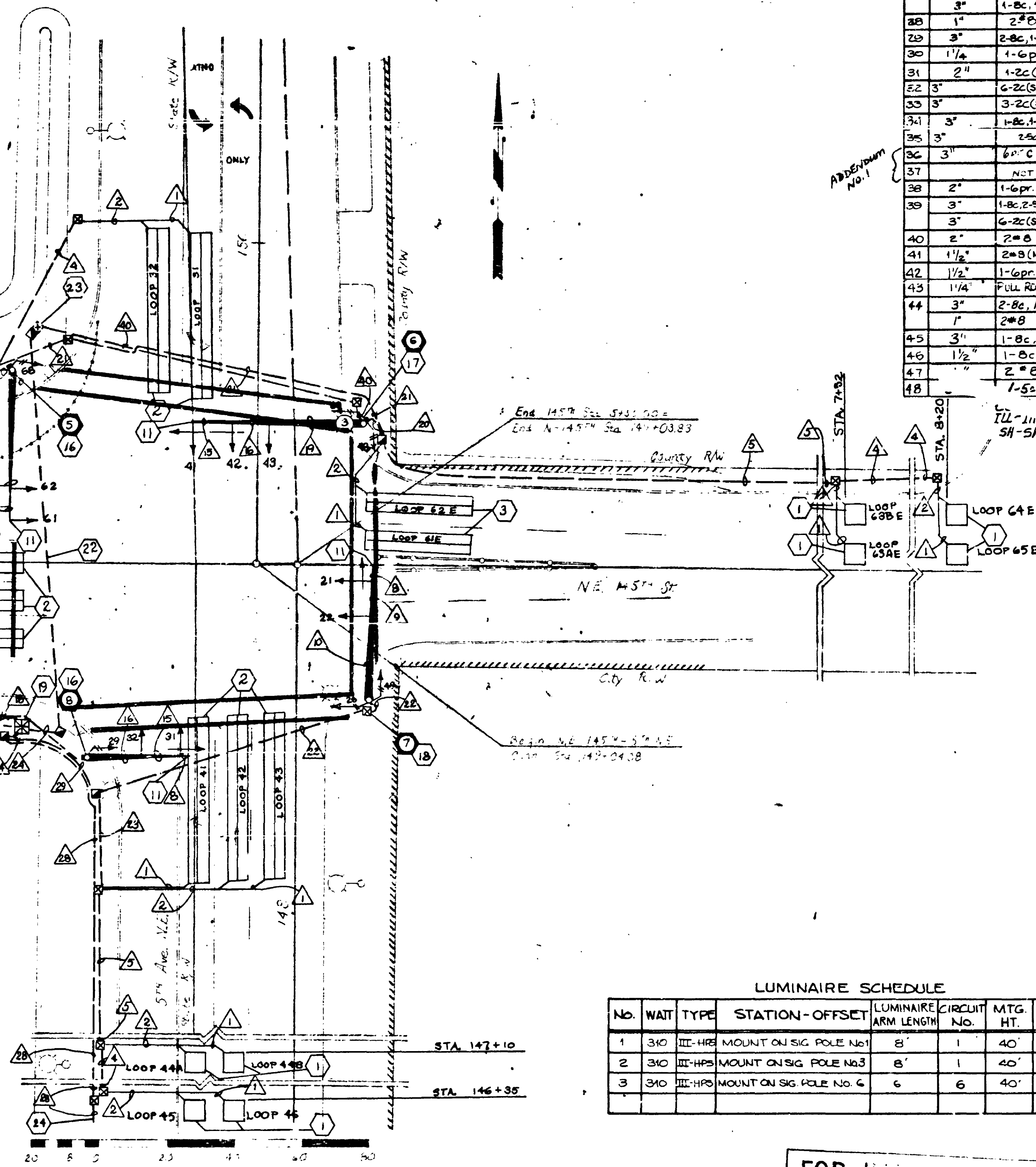
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SH-Shielded

SIGNAL HEAD ASSIGNMENTS - EAST



SIGNAL HEAD ASSIGNMENT NOTES - EAST

1. VEHICLE SIGNAL HEADS SHALL HAVE TUNNEL VISORS. SEE STANDARD PLAN J-6H. EXCEPT: SIGNAL HEAD #22 SHALL HAVE CAP VISORS.
2. ALL VEHICLE SIGNAL HEADS SHALL BE MOUNTED USING MOUNTING TYPE M WITH ELEVATOR FLUMBERIZER LOCATED BETWEEN THE RED AND YELLOW SECTIONS. SEE STANDARD PLAN J-6g.
3. ALL PEDESTRIAN SIGNAL HEADS SHALL USE THE INTERNATIONAL HAND-MAN SYMBOL, BE THE NEON GRID TYPE, AND SHALL HAVE 2 CRATE VISORS.
4. All Vehicle Signal Heads Shall Have 12" Lenses
5. Pedestrian Signal Heads 29, 48 Shall Use Mounting Type B And Pedestrian Signal Heads 28, 49 And 48, 69 Shall Use Mounting Type A.



LUMINAIRE SCHEDULE

| No. | WAT | TYPE | STATION-OFFSET | LUMINAIRE ARM LENGTH | CIRCUIT No. | MTG. HT. | H1 | BASE TYPE | SERVICE NUMBER | LUMIN. VOLTAGE |
|-----|-----|---------|------------------------|----------------------|-------------|----------|-----|-----------|----------------|----------------|
| 1 | 310 | III-HPS | MOUNT ON SIG POLE No.1 | 8' | 1 | 40' | 40' | — | SUA 1124 | 240V. |
| 2 | 310 | III-HPS | MOUNT ON SIG POLE No.3 | 8' | 1 | 40' | 40' | — | SUA 1124 | 240V. |
| 3 | 310 | III-HPS | MOUNT ON SIG POLE No.6 | 6' | 6 | 40' | 40' | — | SUA 675 | 480V. |

FOR FUTURE CONSTRUCTION

| | | | | | | | | | | | |
|-------------|----------------|------------|--------|--------------|------|--------------------|-----------------|-----------|----|--------------|----|
| ENGINEER | DANIEL TAMSKY | REGION NO. | 10 | STATE | WASH | FED. AID PROJ. NO. | IR-IRG-5-3(622) | SHEET NO. | 12 | TOTAL SHEETS | 24 |
| DRAWN | JIM OLSEN | JOB NUMBER | 89W041 | CONTRACT NO. | 3643 | DATE | | REVISION | | BY APPD | |
| REVIEWER | JOHN KERR | | | | | | | | | | |
| CHECKED | HARRY ROWINS | | | | | | | | | | |
| PROJ. ENGR. | CLIFF KUETZWEG | | | | | | | | | | |
| DIST. ADM. | RON BOCKSTRUCK | | | | | | | | | | |

DIVISION OF HIGHWAYS

Washington State
Department of Transportation

SR 5
NE 145TH STREET INTERCHANGE

SIGNAL PLAN - EAST

SHEET
12
OF
24
SHEETS

| SIGNAL SEQUENCE CHART - WEST | | | | | | | | | | | | |
|------------------------------|---------|--------|---------|---------|---------|--------|---------|---------|-----|--------|---------|---------|
| HEAD NO. | φ2 + φ6 | | | | φ1 + φ6 | | | | φ8 | | | |
| | R/W | CHANGE | φ1 + φ6 | φ2 + φ6 | R/W | CHANGE | φ1 + φ6 | φ2 + φ6 | R/W | CHANGE | φ1 + φ6 | φ2 + φ6 |
| 21, 22 | G | Y | Y | Y | R | R | R | R | R | R | R | R |
| 62 | G | G | Y | Y | G | Y | G | G | R | R | R | R |
| 61 | G | G | Y | Y | G | Y | G | G | R | R | R | R |
| 82 | R | R | R | R | R | R | R | R | G | Y | Y | Y |
| 81 | | | | | | | | | G | Y | Y | Y |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| 28, 29 | M | H | H | H | H | H | H | H | H | H | H | DE |
| 68, 69 | M | M | H | H | M | H | M | H | H | H | H | DE |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |

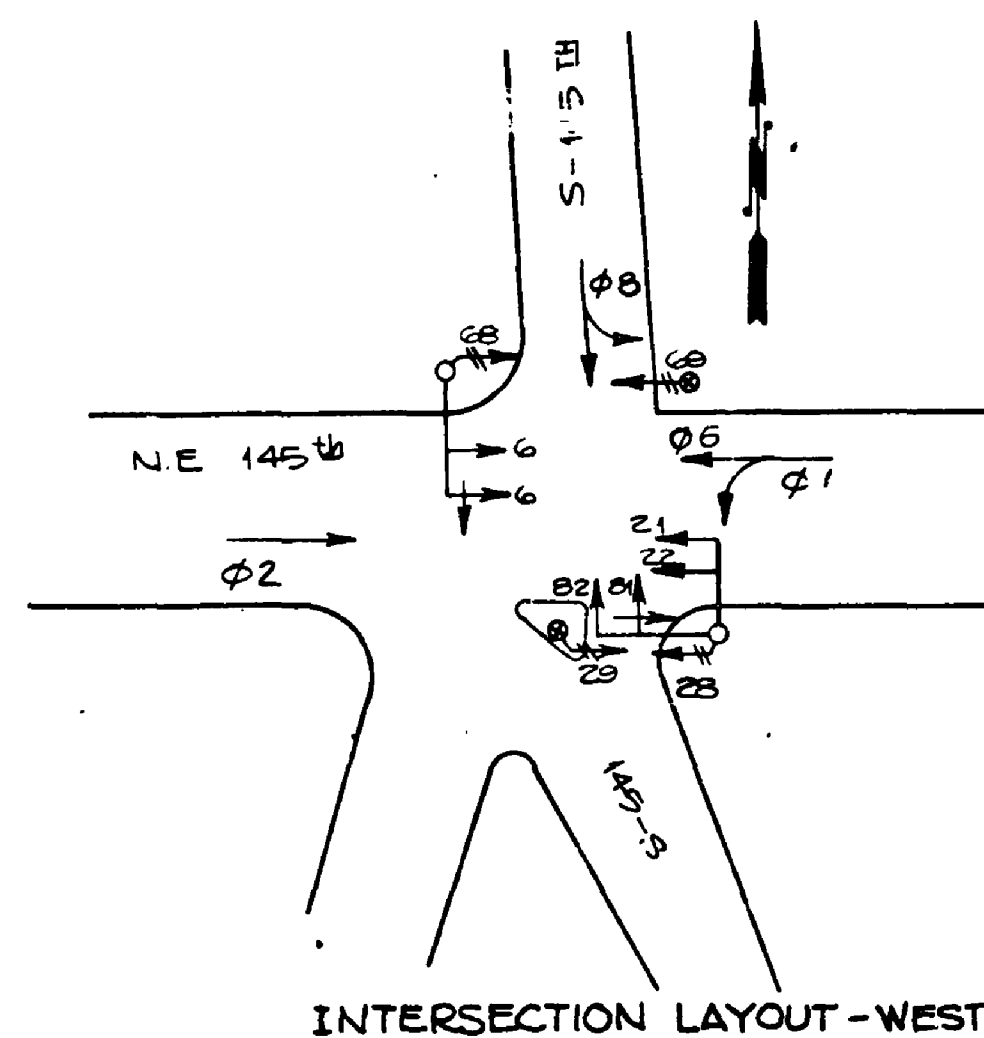
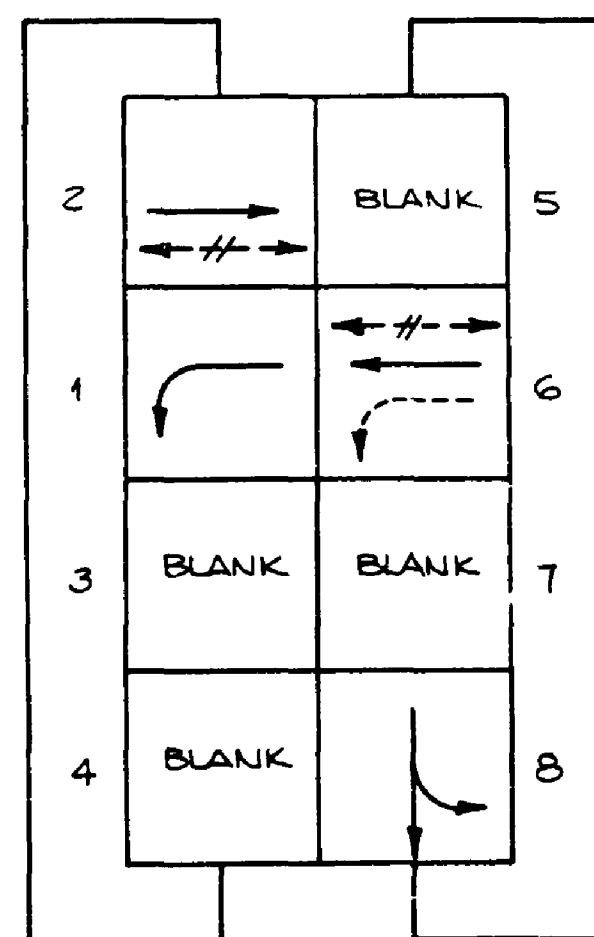
R - RED BALL
Y - YELLOW BALL
G - GREEN BALL
G - GREEN ARROW
H - HAND
M - MAN
DE - DE-ENERGIZE

UPON TERMINATION OF FLASHING OPERATION THE SIGNALS SHALL BE INITIATED IN THE VEHICLE INITIAL INTERVAL OF φ2 + φ6 GREEN.

EMERGENCY PREEMPTION CIRCUITS SHALL BE ASSIGNED AS FOLLOWS:
NE 145TH STREET AND SR5 SOUTHBOUND RAMP

| PLAN NO. | DIRECTION | MOVEMENTS |
|----------|------------|-----------|
| 1 | EASTBOUND | 2 |
| 2 | WESTBOUND | 1, 6 |
| 3 | SOUTHBOUND | 8 |

PHASE SEQUENCE - WEST



INTERSECTION LAYOUT - WEST

| SIGNAL SEQUENCE CHART - EAST | | | | | | | | | | | | |
|------------------------------|---------|--------|---------|----|---------|--------|---------|----|-----|--------|---------|----|
| HEAD NO. | φ2 + φ5 | | | | φ2 + φ6 | | | | φ3 | | | |
| | R/W | CHANGE | φ2 + φ5 | φ3 | R/W | CHANGE | φ2 + φ5 | φ3 | R/W | CHANGE | φ2 + φ5 | φ3 |
| 21 | G | Y | Y | Y | G | Y | Y | Y | R | R | R | R |
| 22 | G | G | Y | Y | G | Y | G | G | R | R | R | R |
| 31, 32 | R | R | R | R | R | R | R | R | G | Y | Y | Y |
| 41, 42, 43 | R | R | R | R | R | R | R | R | R | R | R | R |
| 61, 62 | R | R | R | R | G | Y | Y | Y | R | R | R | R |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| 28, 29 | M | M | H | H | M | H | M | H | H | H | H | DE |
| 48, 49 | H | H | H | H | H | H | H | H | H | H | H | DE |
| 68, 69 | H | H | H | H | M | H | M | H | H | H | H | DE |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |

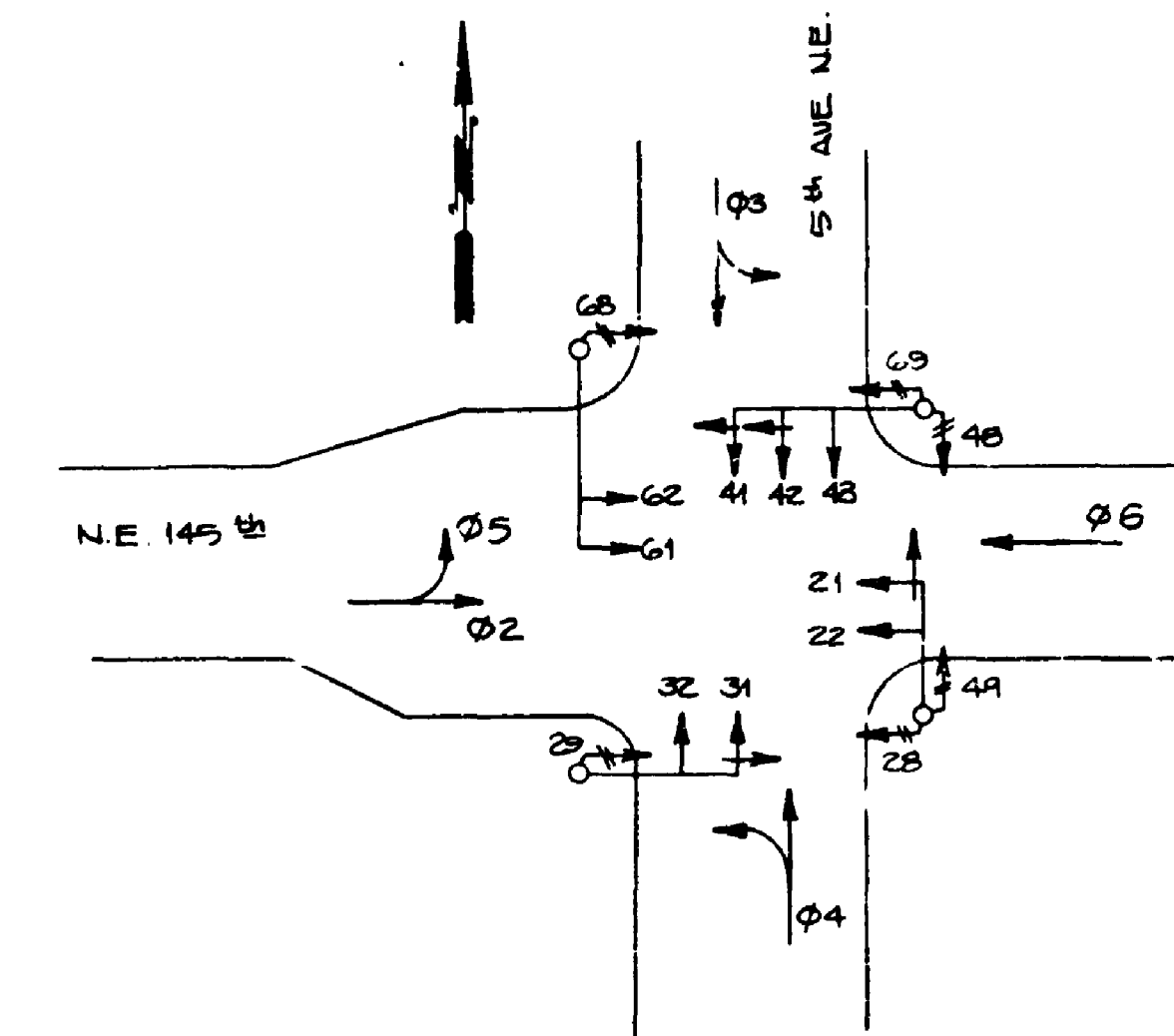
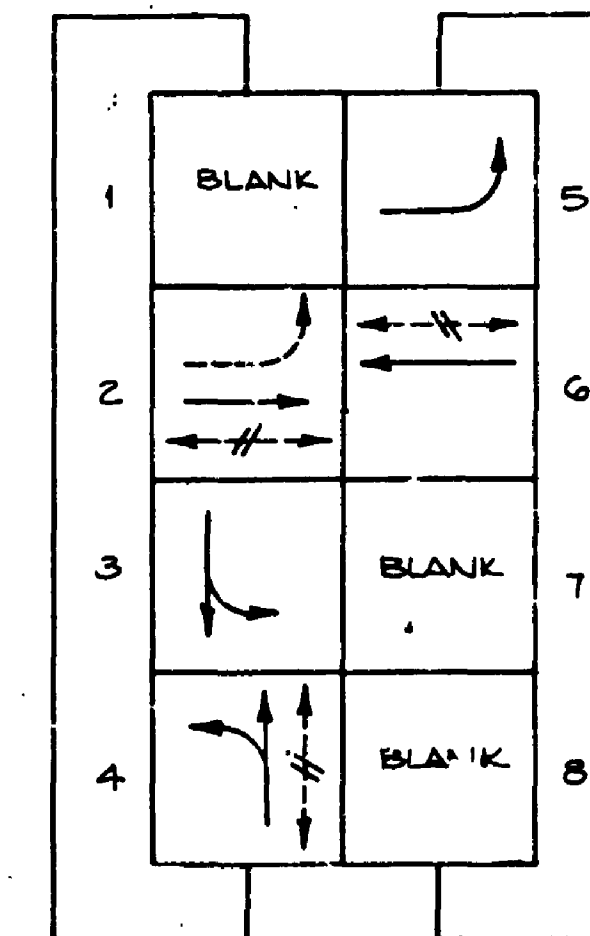
R - RED BALL
Y - YELLOW BALL
G - GREEN BALL
G - GREEN ARROW
H - HAND
M - MAN
DE - DE-ENERGIZE

UPON TERMINATION OF FLASHING OPERATION THE SIGNALS SHALL BE INITIATED IN THE VEHICLE INITIAL INTERVAL OF φ2 + φ5.

EMERGENCY PREEMPTION CIRCUITS SHALL BE ASSIGNED AS FOLLOWS:
NE 145TH ST AND SR5 AVE N.E.

| PLAN NO. | DIRECTION | MOVEMENTS SERVED |
|----------|------------|------------------|
| 1 | EASTBOUND | 2, 5 |
| 2 | WESTBOUND | 6 |
| 3 | SOUTHBOUND | 3 |
| 4 | NORTHBOUND | 4 |

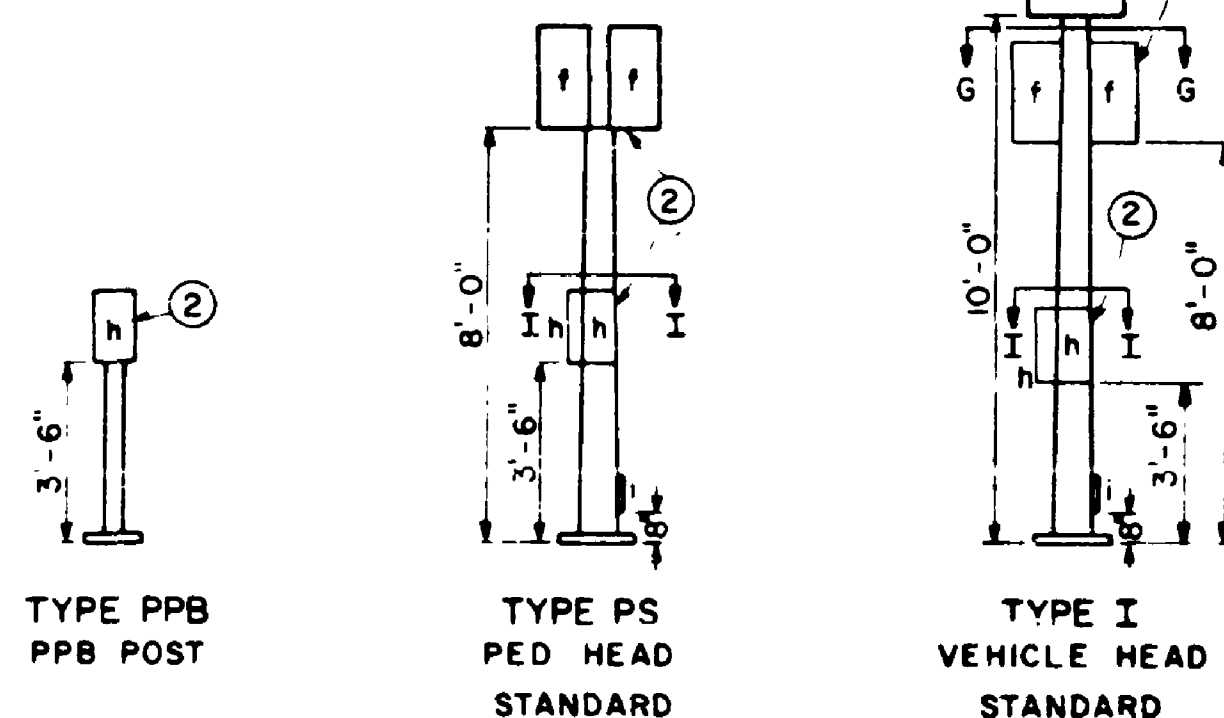
PHASE SEQUENCE - EAST



INTERSECTION LAYOUT - EAST

FOR "AS CONSTRUCTION PLANS" ONLY

| | | | | | | | | | | | | | |
|-------------|----------------|------------|--------|--------------------|-----------------|-----------|-------|--------------|----|----------------------|---|-------------------------------------|----------------|
| ENGINEER | DANIEL TAMSKY | STATE | 10 | FED. AID PROJ. NO. | IR-IRG-5-3(622) | SHEET NO. | 15 | TOTAL SHEETS | 24 | DIVISION OF HIGHWAYS | Washington State Department of Transportation | SR 5 NE 145TH STREET INTERCHANGE | SHEET 15 OF 24 |
| DRAWN | JIM OLSEN | JOB NUMBER | 89W041 | CONTRACT NO. | 3643 | APPROVED | | | | | | SIGNAL SEQUENCE CHART | |
| REVIEWER | JOHN KERR | DATE | | REVISION | | BY | APP'D | | | | | | |
| CHECKED | HARRY ROLLINS | | | | | | | | | | | | |
| PROJ. ENGR. | CLIFF KURTZWEG | | | | | | | | | | | | |
| DIST. ADM. | RON BOCKSTRUCK | | | | | | | | | | | | |

[illegible]

- ## LEGEND
- a Vehicle Signal Head
 - b Sign
 - c Street Name Sign
 - d Pre-Empty Detector
 - e Pre-Empty Indicator
 - f Pedestrian Signal Head
 - g Cabinet
 - h Pedestrian Push Button Assembly
 - i Handhole
 - j Luminaire


NOTES

- ① Mounting couplings installed by fabricator at offset distance indicated in chart
- ② Field installed.
- ③ See Special Provisions for Luminaire Arm Type
- ④ Mount Terminal Cabinet Just Below Pedestrian Heads.

[illegible]

* SEE SPECIAL PROVISIONS
** FUTURE SIGNAL HEAD LOCATIONS.
INSTALL COUPLINGS AT LOCATIONS INDICATED.

FOR "AS CONSIDERED
PLANS" ONLY

| | | | | | | | | | | | | | |
|---------------------------------|--|--|--|----------------------|--------------------|-----------------|--------------|----------------------|----------|--|---|--|--------------------|
| | | | | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS | DIVISION OF HIGHWAYS | |  Washington State Department of Transportation | SR 5 NE 145TH STREET INTERCHANGE | | 16 24 SHEETS |
| DRAWN PHIL FISHER | | | | 10 | WASH | IR-IRG-5-3(622) | | | | | SIGNAL STANDARD DETAIL CHART | | |
| CHECKED JOHN KERR PROJ. ENGR | | | | JOB NUMBER C9W041 | | | | | | | | | |
| DIST. ADM R E Bockstruck | | | | CONTRACT NO. 5643 | | | | | APPROVED | | | | |
| DATE | | | | REVISION | | BY APP'D | | | | | | | |

CABINET

DIMENSIONS SHOWN ARE NOMINAL AND SHALL BE ADJUSTED TO ACCOMMODATE THE VARIOUS SIZES OF EQUIPMENT INSTALLED BY THE CONTRACTOR.

ALL DOORS SHALL BE FULLY GASKETED, ATTACHED BY FULL LENGTH STAINLESS STEEL PIANO HINGES AND PROVIDED WITH PADLOCKABLE VAULT HANDLES. HINGES SHALL HAVE BRASS PINS. THE GREATER PANEL COMPARTMENT DOOR SHALL BE FURNISHED WITH A THREE-PINE, SPRING-LOADED HITCH AND A BEST CX CONSTRUCTION CORE. THE METER COMPARTMENT DOOR SHALL BE PROVIDED WITH TWO 4' X 6' POLISHED WIRE GLASS WINDOWS.

METERING ARRANGEMENTS MAY VARY WITH DIFFERENT SERVING UTILITIES.
THE CONTRACTOR SHALL VERIFY THE REQUIREMENTS WITH THE UTILITY
PRIOR TO FABRICATING THE SERVICE EQUIPMENT.

ALL BUSSWORK SHALL BE HIGH GRADE COPPER AND SHALL EQUAL OR EXCEED THE MAIN BREAKER RATING. ALL CIRCUIT BREAKERS SHALL BOLT TO THE BUSSWORK. JUMPERING OF BREAKERS IS NOT ALLOWED.

ALL BREAKERS AND CONTACTORS WITHIN THE SERVICE ENCLOSURE SHALL HAVE APPROPRIATELY ENGRAVED PHENOLIC NAMEPLATES IDENTIFYING THE RESPECTIVE CIRCUITS. THE NAMEPLATES SHALL BE ATTACHED BY EITHER SCREW OR RIVET. FOR BREAKER RATING AND CONTACTOR SIZES, SEE THE EQUIPMENT SCHEDULE THIS SHEET.

ALL INTERNAL WIRE RUNS SHALL BE IDENTIFIED WITH TO-FROM CODED TAGS LABELLED WITH NUMBERS SHOWN ON THE SCHEDULE. APPROVED PVC WIRE MARKING SLEEVES SHALL BE USED FOR THIS PURPOSE.

ALL WIRE SUPPLYING POWER TO THE STRIP HEATER SHALL BE HEAT RESISTANT TYPE.

PHOTOELECTRIC CONTROL, WHEN SPECIFIED, SHALL MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS.
















THE PHOTOCELL ENCLOSURE SHALL BE FABRICATED FROM 5/8 INCH EXPANDED STEEL MESH WITH WELDED SEAMS AND MOUNTING FLANGES. THE ENCLOSURE SHALL BE HOT DIP GALVANIZED AFTER FABRICATION AND FASTENED TO THE TOP OF THE SERVICE CABINET WITH A MINIMUM OF 4 - 1/4" X 1" MACHINE BOLTS WHICH ARE SPOT WELDED TO THE TOP OF THE CABINET.

ALL ELECTRICAL CONNECTIONS BETWEEN THE PHOTOCELL AND THE SERVICE PANEL SHALL BE MADE THROUGH WATERTIGHT HUBS AND BUSHINGS. FLEXIBLE CONDUIT SHALL BE USED BETWEEN THE PHOTOCELL AND THE BREAKER PANEL COMPARTMENT WHEN THE PHOTOCELL IS MOUNTED OVER THE METER COMPARTMENT.

PROVISIONS SHALL BE MADE TO ENABLE THE PHOTOCCELL TO BE ROTATED AND SECURED IN FINAL POSITION AFTER INSTALLATION.



SERVICE CABINET LEGEND

- | | | |
|---|---|---|
|  1 Meter Base |  8 Neutral Bus |  2 |
|  2 Main Breaker |  9 Control Breaker | |
|  3 Auxiliary Breaker |  10 Receptacle Breaker (GFI SPST, 125 V. *T* Rated) | |
|  4 Branch Breaker |  11 Receptacle Ground 20 Amp | |
|  5 Test Switch (SPST, 10 Amp, 125 V. *T* Rated) |  12 Heater Breaker (SPST, 125 V.) | |
|  6 Photoelectric Control |  13 Thermostat | |
|  7 Contactor |  14 Strip Heater (000 Watt Nominal) | |



FOR "A" STER

120 / 240 VOLT SERVICE CABINET DETAILS
SEE STD PLAN J-3b FOR FOUNDATION DETAILS

| | | | | | | | | | | | | |
|---|-------------|----------------|------|------|----------|--------------|-------|---------------------|----------------------|--|-------------------------------------|-----------------------------------|
| 6 | | | | | | REGION NO. | STATE | FED. AID. PROJ. NO. | DIVISION OF HIGHWAYS | WASHINGTON STATE DEPARTMENT OF TRANSPORTATION | SR 5 NE 145TH STREET INTERCHANGE | SHEET 17 OF 24 SHEETS |
| 5 | DESIGNED BY | | | | | 10 | WASH | IR-IRG-5-3(622) | | | | |
| 4 | ENTERED BY | E.F.WELCKE | | | | JOB NUMBER | | | | | | |
| 3 | CHECKED BY | V.O.DANG | | | | 89W041 | | | | | | |
| 2 | PROJ. ENGR. | | | | | CONTRACT NO. | | | | | | |
| 1 | DIST. ADM. | R.E.BÖCKSTRUCK | DATE | DATE | REVISION | BY | | | APPROVED | | MODIFIED TYPE "D" SERVICE DETAILS | |

SIGN SPECIFICATIONS

ROADSIDE SIGN STRUCTURES


| SIGN NO. | SIGN CODE NUMBER | LOCATION | SIGN SIZE X | SIZE Y | LETTER SIZE OR CODE | POST MATERIAL | POST SIZE | POST LENGTH H1 H2 H3 H4 | | | | CLEARANCE V W | | REMARKS |
|----------|-------------------------|----------------|----------------|-----------|------------------------|---------------|-----------|----------------------------|--|--|--|------------------|------|---------------------------------------|
| 1 | ONE WAY | 145TH 9+59B L. | 36" | 12" | | SIG. POLE | | | | | | 12' | 7' | INSTALL SIGN JUST ABOVE PED. HEAD |
| 2 | DO NOT ENTER | 145TH 9+71B L. | 36" | 36" | STD. | WOOD | 4X4 | 14' | | | | 7' | 15' | RELOCATE SIGN TO NEW WOOD POST |
| 3 | I-5 SOUTH | 145TH 0+04 R | 36" | 36" | | WOOD | 4X6 | 17' | | | | 7.6' | 3.5' | RELOCATE SIGN TO NEW WOOD POST |
| 4 | R5-1 (DO NOT ENTER) | 5-145 150+32 R | 36" | 36" | STD. | WOOD | 4X4 | 14' | | | | 7' | 6' | SIGN INSTALLATION ANGLE = +80 DEG. |
| 5 | R5-1A (WRONG WAY) | 5-145 154+32 R | 36" | 24" | STD. | WOOD | 4X4 | 10' | | | | 5' | 6' | |
| 6 | R5-1 (ONE WAY) | 145TH 0+29 L | 36" | 12" | STD. | SIG. POLE | | | | | | 12' | 6' | INSTALL SIGN JUST ABOVE PED. HEAD |
| 7 | R5-1 (ONE WAY) | 145TH 0+29 L | 36" | 12" | STD. | SIG. POLE | | | | | | 7' | 6' | MOUNT ON BACKSIDE OF S. #5 |
| 8 | D3-301 (NE 145TH ST.) | 145TH 0+49 R | 84" | 12" | 6"D | MAST ARM | | | | | | | | MOUNT ON SIGNAL POLE #3 MAST ARM |
| R9 | CHANGEABLE MESSAGE | 145TH 0+61 R | | | | SIG. POLE | | | | | | | | RETURN SIGN AND ASSEMBLY TO SIG. SHOP |
| R10 | PARK AND RIDE | 145TH 0+97 R | | | | EX. WOOD | | | | | | | | REMOVE SIGN AND POST |
| R11 | NO PARKING | 145TH 0+97 R | | | | EX. WOOD | | | | | | | | REMOVE SIGN |
| 12 | D4-2 (PARK AND RIDE) | 145TH 0+97 R | 36" | 48" | STD. | WOOD | 6X8 | 18' | | | | 10'4" | 6' | MOUNT BEHIND BRIDGE RAILING |
| 13 | R8-3A (NO PARKING) | 145TH 0+97 R | 24" | 24" | STD. | | | | | | | 8'4" | 6' | MOUNT BELOW SIGN #12 |
| 14 | R8-3AP (ON BRIDGE) | 145TH 0+97 R | 24" | 16" | STD. | | | | | | | 7' | 6' | MOUNT BELOW SIGN #13 |
| 15 | M8-101 (I-5 NORTH) | 145TH 2+30 R | 36" | 60" | 6"D | LUM. POLE | | | | | | 7' | 6' | MOUNT ON EXISTING LUMINAIRE POLE |
| 16 | M8-101 (I-5 SOUTH) | 145TH 3+22 L | 36" | 60" | 6"D | LUM. POLE | | | | | | 7' | 6' | MOUNT ON EXISTING LUMINAIRE POLE |
| 17 | M8-101 (513' SOUTH) | 145TH 7+80 L | 36" | 60" | 6"D | WOOD | 4X6 | 16' | | | | 7' | 4' | |
| 18 | D3-301 (5TH AVE. N.E.) | 145TH 4+71 L | 78" | 12" | 5"D | MAST ARM | | | | | | | | MOUNT ON SIGNAL POLE #5 MAST ARM *** |
| 19 | D3-301 (N.E. 145TH ST.) | 145TH 4+94 R | 84" | 12" | 6"D | MAST ARM | | | | | | | | MOUNT ON SIGNAL POLE #8 MAST ARM *** |
| 20 | SHORELINE COMM. COLLEGE | 145TH 5+75 L | | | | SIG. POLE | | | | | | 12' | 3' | RELOCATE SIGN TO JUST ABOVE PED. HEAD |
| 21 | D3-301 (N.E. 145TH ST.) | 145TH 5+75 L | 84" | 12" | 6"D | MAST ARM | | | | | | | | MOUNT ON SIGNAL POLE #6 MAST ARM *** |
| 22 | TO I-5 NORTH | 145TH 5+86 L | | | | WOOD | 6X6 | 18' | | | | 7' | 4' | |

NOTES: POST LENGTHS SHOWN ARE APPROXIMATE. FINAL VALUES SHALL BE DETERMINED IN THE FIELD PRIOR TO FABRICATION.
 FOR STRUCTURE AND MOUNTING DETAILS SEE STANDARD PLAN SHEET SERIES G.
 FOR CODE REFERENCES AND STANDARD SIGN LAYOUT DETAILS SEE WASHINGTON STATE "SIGN FABRICATION MANUAL".

* SEE SHEET 16, MOUNT PER STD. PLAN J-6H SIGN TO FACE NORTH.
 *** SEE SHEET 16, MOUNT PER STD. PLAN J-6H.

FOR "AS CONSTRUCTED
PLANS" ONLY

DEITZEN CORPORATION 105519

| | | | | | | | | | | | | | | | | | | | |
|-------|---------|-------------|-----------|------|----------|----------|------|-------|---------|--------------------|------------------|------------|--------|--------------|------|------------------|--|--|-----------------------------------|
| DRAWN | CHECKED | PROJ. ENGR. | CITY ADM. | DATE | REVISION | BY APP'D | 3643 | STATE | 10 WASH | FED. AID PROJ. NO. | IR-IRG-5-3 (622) | JOB NUMBER | 89W041 | CONTRACT NO. | 3643 | HIGHWAY DIVISION |  Washington State Department of Transportation | SR 5 NE 145TH STREET INTERCHANGE SIGN SPECIFICATIONS | SHEET 18 OF 24 POINTS |
|-------|---------|-------------|-----------|------|----------|----------|------|-------|---------|--------------------|------------------|------------|--------|--------------|------|------------------|--|--|-----------------------------------|

DOT FORM 221-024
R/9/86

SR 5 / 2559

SIGN SPECIFICATIONS


ROADSIDE SIGN STRUCTURES

[illegible]

NOTES: POST LENGTHS SHOWN ARE APPROXIMATE. FINAL VALUES SHALL BE DETERMINED IN THE FIELD PRIOR TO FABRICATION.
FOR STRUCTURE AND MOUNTING DETAILS SEE STANDARD PLAN SHEET SERIES G.
FOR CODE REFERENCES AND STANDARD SIGN LAYOUT DETAILS SEE WASHINGTON STATE "SIGN FABRICATION MANUAL".

* * SEE SHEET 16 , MOUNT PER STD. PLAN J-6H.

FOR "AS CONSTRUCTED
PLANS" ONLY

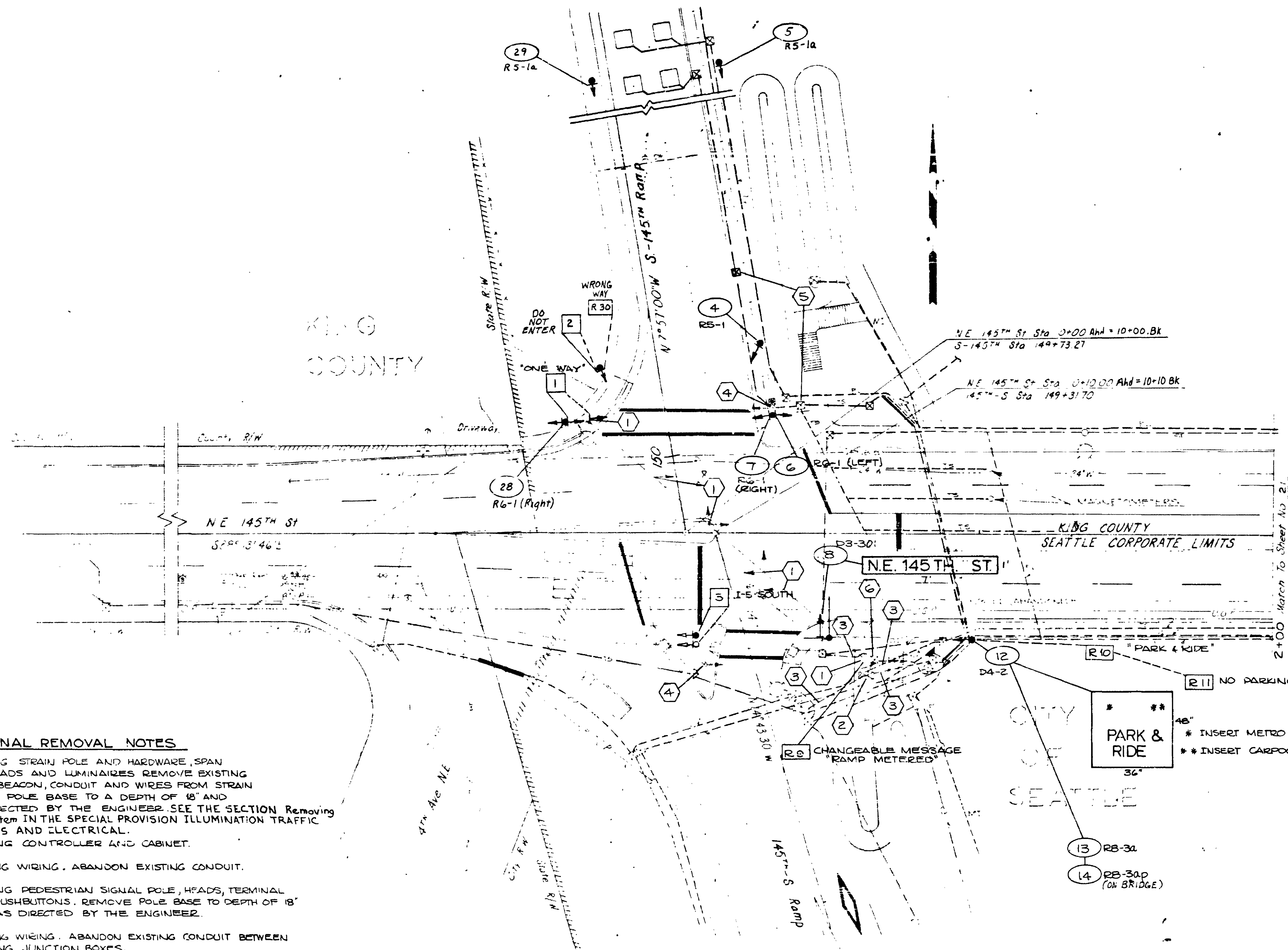
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|-------------|--|--|--------------|--|--|----------|--|--|-----------------|--|--|------------------|--|--|--|--|--|---|--|--|---|--|--|
| DRAWN | | | 10 | | | WASH | | | IR-IRG-5-3(422) | | | HIGHWAY DIVISION | | |  Washington State Department of Transportation | | | SR 5 NE 145TH STREET INTERCHANGE | | | DATE 19 00 24 WEST | | |
| CHECKED | | | JOB NUMBER | | | 89W041 | | | | | | | | | | | | | | | | | |
| PROJ. ENGR. | | | CONTRACT NO. | | | 3643 | | | | | | | | | | | | | | | | | |
| DIST. ADM. | | | DATE | | | REVISION | | | STAFF | | | APPROVED | | | | | | | | | | | |

DOT FORM 221-084
R/9/86

19
24
SHEETS

525/2559

T. 26N. R. 4E. W.M.



- LEGEND**
- UGP Underground Power
 - W Waterline
 - SM Sewer Manhole
 - F Fence
 - TJ Type I Junction Box
 - V Valve
 - C Controller
 - L Luminaire
 - NG Natural Gas Line
 - UP Utility Pole
 - BEC Electric Box
 - CR Curb Ramp
 - PS Pedestrian Signal Pole
 - CB Catch Basin
 - WM Water Manhole
 - PND Pacific Northwest Bell-Electrical Ducts
 - FH Fire Hydrant
 - SW Wire and/or
 - TJ Type II Junction Box
 - CR Curb Ramp
 - WM Water Manhole

SIGNAL REMOVAL NOTES

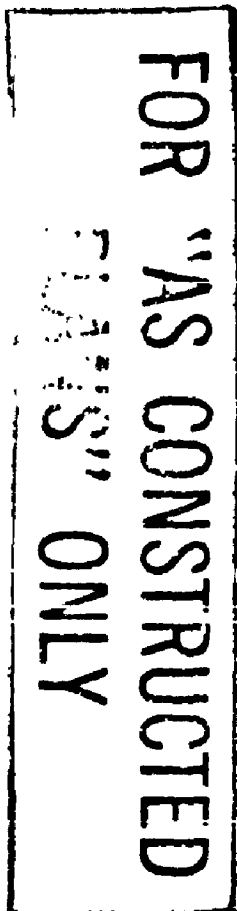
1. REMOVE EXISTING STRAIN POLE AND HARDWARE, SPAN WIRE, SIGNAL HEADS AND LUMINAIRES REMOVE EXISTING SERVICE, SIGN, BEACON, CONDUIT AND WIRES FROM STRAIN POLE REMOVE POLE BASE TO A DEPTH OF 18" AND BACKFILL AS DIRECTED BY THE ENGINEER. SEE THE SECTION Removing Existing Signal System IN THE SPECIAL PROVISION ILLUMINATION TRAFFIC SIGNAL SYSTEMS AND ELECTRICAL.
2. REMOVE EXISTING CONTROLLER AND CABINET.
3. REMOVE EXISTING WIRING, ABANDON EXISTING CONDUIT.
4. REMOVE EXISTING PEDESTRIAN SIGNAL POLE, HEADS, TERMINAL CABINET AND PUSHBUTTONS. REMOVE POLE BASE TO DEPTH OF 18" AND BACKFILL AS DIRECTED BY THE ENGINEER.
5. REMOVE EXISTING WIRING, ABANDON EXISTING CONDUIT BETWEEN NEW AND EXISTING JUNCTION BOXES.
6. COIL EXISTING 3C(SH) SIGN CONTROL CABLE IN THIS JUNCTION BOX AND SEAL SIGN CONTROL CABLE ENDS WITH AN APPROVED HEAT SHRINK END CAP.


- CONDUIT SYSTEM**
- 1. METRO TRANSIT SIGNALS
 - 2. EXISTING TRAFFIC SIGNALS
 - 3. TYPE II JUNCTION BOXES
 - 4. TYPE II JUNCTION BOXES
 - 5. TYPE II JUNCTION BOXES
 - 6. TYPE II JUNCTION BOXES
 - 7. TYPE II JUNCTION BOXES
 - 8. TYPE II JUNCTION BOXES

FOR PROJECTED

| | | | | | | | | | | | | | |
|----------------------------|--|-------------------|--|------------------------------------|--|----------------------|--|---|--|-----------------------------------|--|-----------------------|--|
| ENGINEER DANIEL TAMSKY | | STATE 10 | | FED. AID PROJ. NO. IR-IRG-5-3(622) | | DIVISION OF HIGHWAYS | | Washington State Department of Transportation | | SR 5 NE 145TH STREET INTERCHANGE | | SHEET 20 OF 24 SHEETS | |
| DRAWN JIM OLSEN | | JOB NUMBER 89W041 | | CONTRACT NO. 2559 | | APPROVED | | | | SIGNAL REMOVAL AND SIGNING - WEST | | | |
| REVIEWER JOHN KERR | | DATE | | REVISION | | BY APP'D | | | | | | | |
| CHECKED HARRY ROLLINS | | | | | | | | | | | | | |
| PROJ. ENGR. CLIFF KURTZWEG | | | | | | | | | | | | | |
| DIST. ADM. RON BOCKSTUCK | | | | | | | | | | | | | |

Scale in Feet

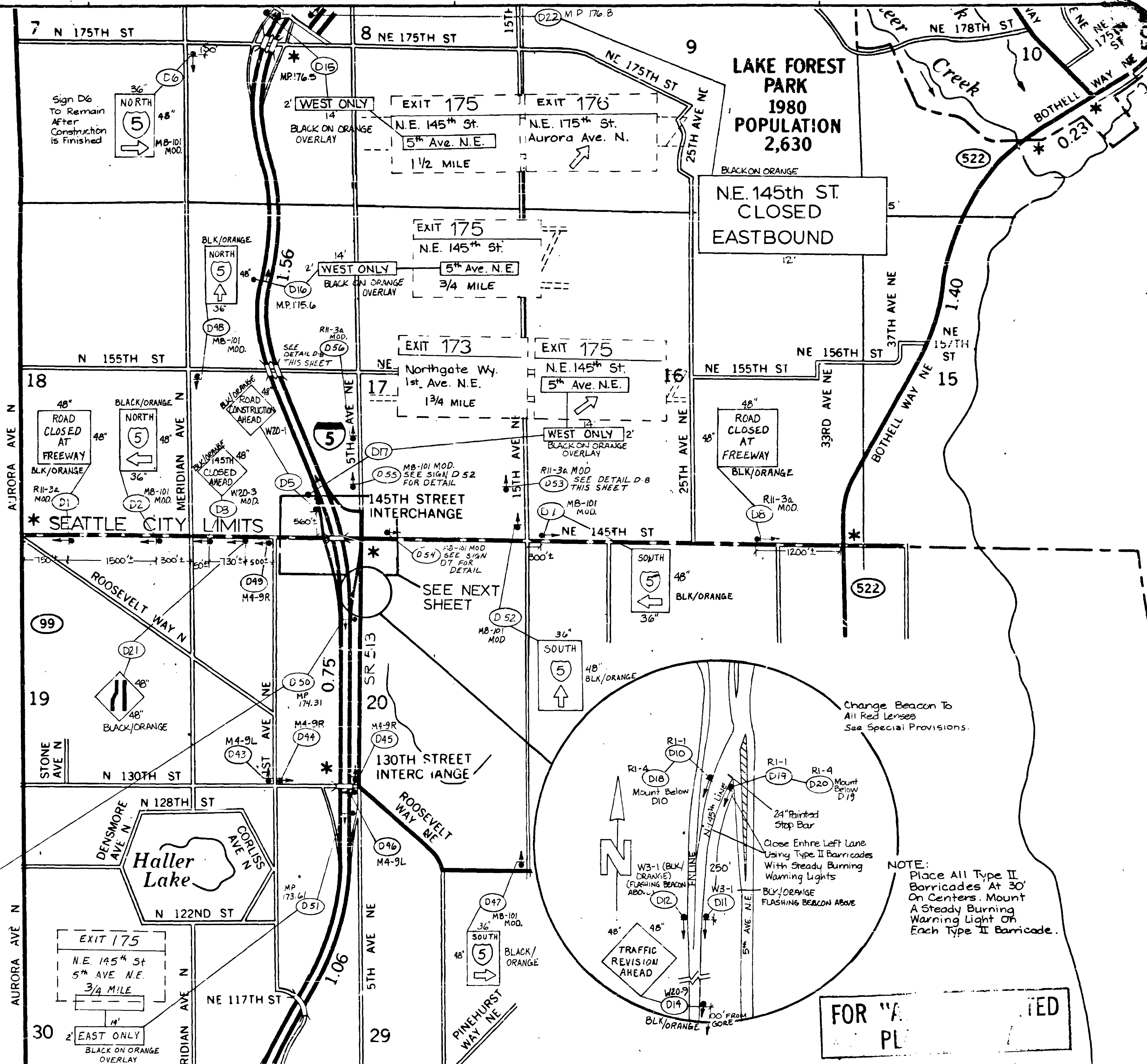
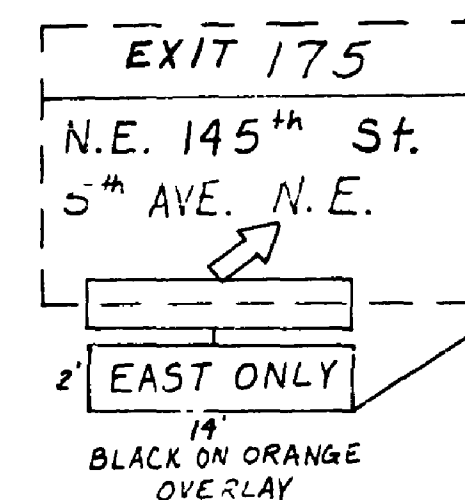


| | | |
|--|--|--|
| <div style="text-align: center;">DIVISION OF HIGHWAYS</div> <div style="text-align: center;">  <div style="display: inline-block; vertical-align: middle;"> Washington State Department of Transportation </div> </div> <div style="text-align: center;">APPROVED</div> | <div style="text-align: center;">SR 5</div> <div style="text-align: center;">NE 145TH STREET INTERCHANGE</div> | <div style="text-align: center;">SHEET</div> <div style="text-align: center;">21</div> <div style="text-align: center;">OF</div> <div style="text-align: center;">24</div> <div style="text-align: center;">SHEETS</div> |
| | <div style="text-align: center;">SIGNAL REMOVAL AND SIGNING- EAST</div> | |

| CONSTRUCTION SIGNING | | | | | |
|----------------------|------------------------------------|-----------------|-------------|-------------|-------------|
| NO. & CODE | DESCRIPTION | LETTER SIZE | LETTER CODE | SIGN SIZE X | SIGN SIZE Y |
| D1 RI-3a MOD. | ROAD CLOSED AT FREEWAY | 6" | C | 4' | 4' |
| D2 MB-101 MOD. | ← TO I-5 NORTH | 6" (24" SHIELD) | D | 3' | 4' |
| D3 W20-3 MOD. | 145TH CLOSED AHEAD | STD. | | 4' | 4' |
| D5 W20-1 | ROAD CONSTRUCTION AHEAD | STD. | | 4' | 4' |
| D6 MB-101 MOD. | TO I-5 → | STD. | | 3' | 4' |
| D7 MB-101 MOD. | ← TO I-5 SOUTH | 5" (18" SHIELD) | D | 3' | 4' |
| D8 RI-3a MOD. | ROAD CLOSED AT FREEWAY | 6" | C | 4' | 4' |
| D10 RI-1 | STOP | STD. | | 4' | 4' |
| D11 W3-1 | STOP AHEAD | STD. | | 4' | 4' |
| D12 W3-1 | STOP AHEAD | STD. | | 4' | 4' |
| D14 W20-9 | TRAFFIC REVISION AHEAD | STD. | | 4' | 4' |
| D15 | "WEST ONLY" PANEL | 12" | E | 14' | 2' |
| D16 | "WEST ONLY" PANEL | 12" | E | 14' | 2' |
| D17 | "WEST ONLY" PANEL | 12" | E | 14' | 2' |
| D18 RI-4 | "ALL WAY" PLAQUE | STD. | | 18" | 6" |
| D19 RI-1 | STOP | STD. | | 4' | 4' |
| D20 RI-4 | "ALL WAY" PLAQUE | STD. | | 18" | 6" |
| D21 W4-2 | PAVEMENT WIDTH TRANSITION | STD. | | 4' | 4' |
| D22 | N.E. 145th STREET CLOSED EASTBOUND | 10" | D | 12' | 5' |
| D43 M4-9L | DETOUR ← | 5" | D | 30" | 24" |
| D44 M4-9R | DETOUR → | 5" | D | 30" | 24" |
| D45 M4-9R | DETOUR → | 5" | D | 30" | 24" |
| D46 M4-9L | DETOUR ← | 5" | D | 30" | 24" |
| D47 MB-101 MOD. | → TO I-5 SOUTH | 5" | D | 3' | 4' |
| D48 MB-101 MOD. | ↑ TO I-5 NORTH | 5" | D | 3' | 4' |
| D49 4-9R | DETOUR → | 5" | D | 30" | 24" |
| D50 | "EAST ONLY" PANEL | 12" | E | 14' | 2' |
| D51 | "EAST ONLY" PANEL | 12" | E | 14' | 2' |
| D52 MB-101 MOD. | TO I-5 SOUTH ↑ | 5" | D | 3' | 4' |
| D53 RI-3a MOD. | ROAD CLOSED AT FREEWAY | 6" | C | 4' | 4' |
| D54 MB-101 MOD. | TO I-5 SOUTH ← | 5" | D | 3' | 4' |
| D55 MB-101 MOD. | TO I-5 SOUTH ↑ | 5" | D | 3' | 4' |
| D56 RI-3a MOD. | ROAD CLOSED AT FREEWAY | 6" | C | 4' | 4' |

NOTE: THE CONTRACTOR SHALL FURNISH ALL CONSTRUCTION SIGNING AND SHALL BE PAID LUMP SUM UNDER THE ITEM "CONSTRUCTION/DETOUR SIGNING" (SEE SPECIAL PROVISIONS).

SEE SHEET 23 FOR ADDITIONAL CONSTRUCTION SIGNING



| | | | | | | | | | |
|------------------------------|----------|-------------------|------|------------------------------------|--|--------------|--|---|--|
| DRAWN - K. JOHNSON | | STATE: WASH | | FED. AID PROJ. NO. IR-IRG-5-3(622) | | SHEET NO. 22 | | TOTAL SHEETS 24 | |
| REVIEWED - J. KERR | | JOB NUMBER 89W041 | | CONTRACT NO. 3643 | | APPROVED | | Washington State Department of Transportation | |
| CHECKED - D. TANSKY | | | | | | | | SR 5 | |
| PROJ. ENGR. | | | | | | | | NE 145TH STREET INTERCHANGE | |
| DIST. ADM. - R.E. BOCKSTRUCK | | | | | | | | CONSTRUCTION SIGNING | |
| DATE | REVISION | BY | APPD | | | | | | |

6 TYPE III BARRICADES 8' LONG
WITH FLASHING YELLOW LIGHTS
(3 RIGHT-3 LEFT, SILVER ON ORANGE).
SEE SHEET 1 FOR DETAILS.

Type II Barricades
w/ Steady Burning
Warning Lights

Type II Barricades
Steady Burning
Warning Lights.

6 TYPE III BARRICADES 8' LONG
WITH FLASHING YELLOW LIGHTS
(3 RIGHT-3 LEFT, SILVER ON ORANGE).
SEE SHEET 1 FOR DETAILS.

| No. | CODE | DESCRIPTION | LETTER SIZE | LETTER CODE | SIGN X | SIGN Y |
|-----|----------------|----------------------------|-----------------|-------------|--------|--------|
| D4 | R3-7 | Right Lane Must Turn Right | Std. | | 4' | 4' |
| D23 | W20-3 | 145TH Closed Ahead | Std. | | 4' | 4' |
| D9 | Mod | Local Access Only | 6" | C | 3' | 3' |
| D24 | M8-101 MOD. | ← I-5 South | 5" (18" Shield) | D | 3' | 48" |
| D25 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D26 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D27 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D28 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D29 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D30 | R3-2 | No Left Turn | Std. | | 36" | 36" |
| D31 | R3-2 | No Left Turn | Std. | | 36" | 36" |
| D32 | R3-1 | No Right Turn | Std. | | 36" | 36" |
| D33 | R3-2 | No Left Turn | Std. | | 36" | 36" |
| D34 | R3-2 | No Left Turn | Std. | | 36" | 36" |
| D35 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D36 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D37 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D38 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D39 | R3-7 | Right Lane Must Turn Right | Std. | | 48" | 48" |
| D40 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D41 | R11-2 | Road Closed | Std. | | 48" | 30" |
| D42 | R11-2 | Road Closed | Std. | | 48" | 30" |

Signal Note:
Turn Signal
off And Bag
the Heads

The diagram illustrates the placement of a Type II barricade for a lane closure. It shows a cross-section of a road with two lanes: a 'Lane Closed' on the left and a 'Lane Open' on the right. A 'Lane Line' is marked on the right side. A 'Type II Barricade' is positioned at the edge of the closed lane, with a 'Burn Warning Light' mounted on top. The light is angled towards the closed lane. A vertical dimension line indicates a height of '2'' for the light. A note specifies: 'Note: Place All Type II Barricades At 30' On Centers, Mount A Steady Burning Warning Light On Each Type II Barricade.'

TYPE II BARRICADE PLACEMENT DETAIL

A horizontal scale bar with markings at 50, 0, 50, 100, 150, and 200 feet. The text "SCALE IN FEET" is centered below the bar.

FOR

SEE SHEET 22 FOR ADDITIONAL SIGNING

DIVISION OF HIGHWAYS



Washington State
Department of Transportation

SR 5
NE 145TH STREET INTERCHANGE

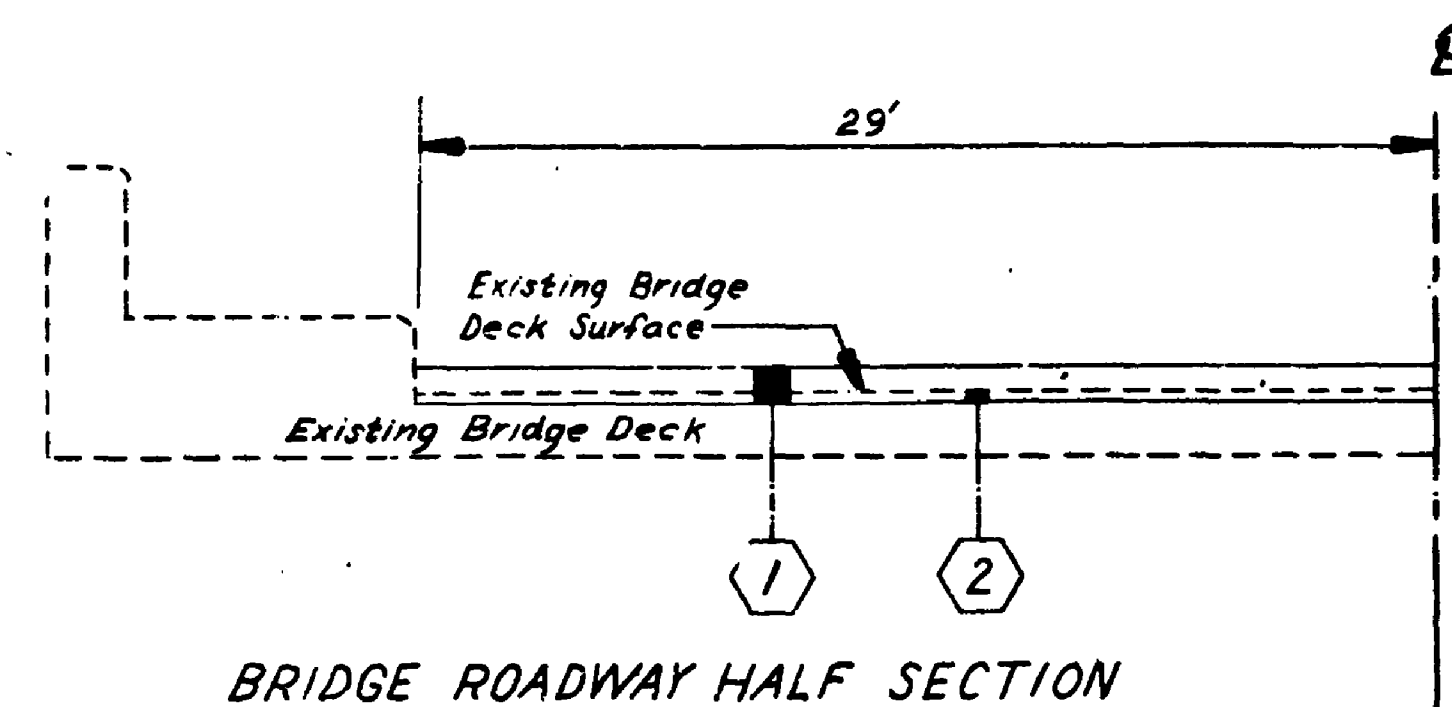
CONSTRUCTION CHANNELIZATION AND SIGNING

SHEET
23
OF
24
SHEETS

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K-E DOT FORM 221-011
Revised 6, 80

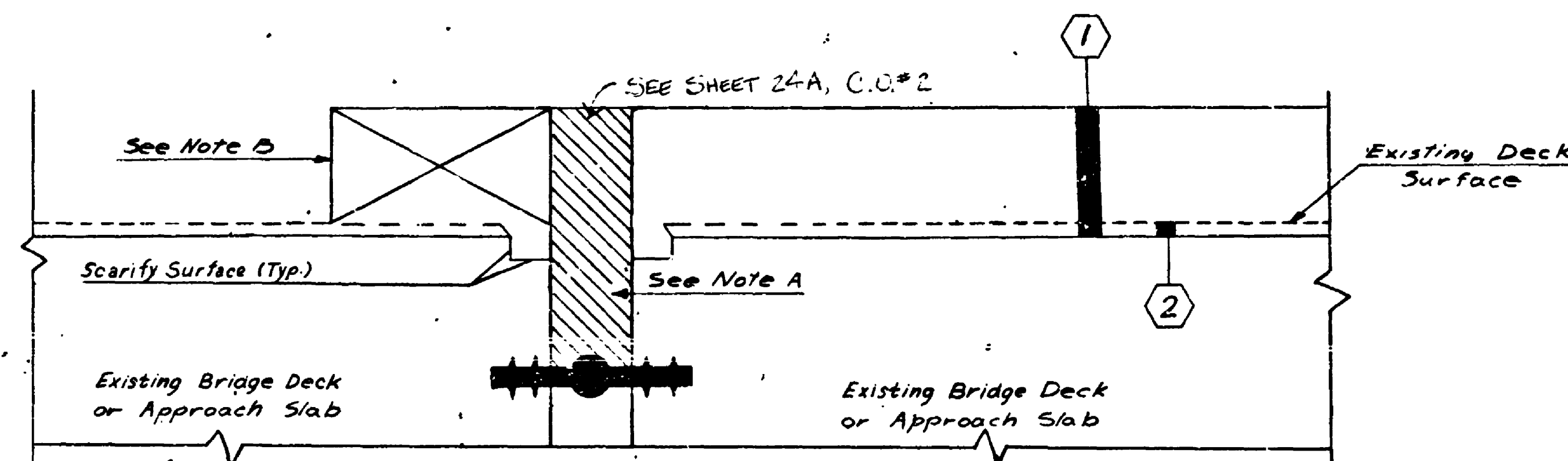
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BRIDGE ROADWAY HALF SECTION

N.E. 145th St. Sta. 0+85.27 to Sta. 3+34.27

BRIDGE 513/32



CONTRACTION JOINT DETAIL

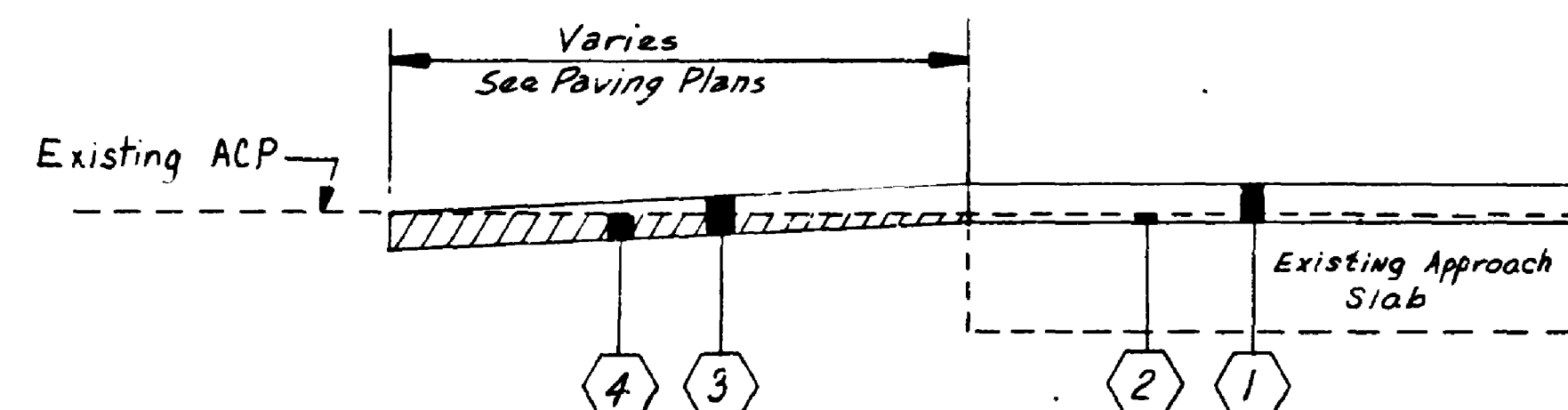
NOTES: A. Remove Existing Joint Filler to The Waterstop Level. Replace with Premolded Joint Filler to The Top of The High Early Strength Latex Modified Concrete Overlay.

B. Wood Backing Strip to Be Removed after Overlay Material Placed on Other Side of Joint Has Taken Initial Set.

C. See The Special Provision CONTRACTION JOINT

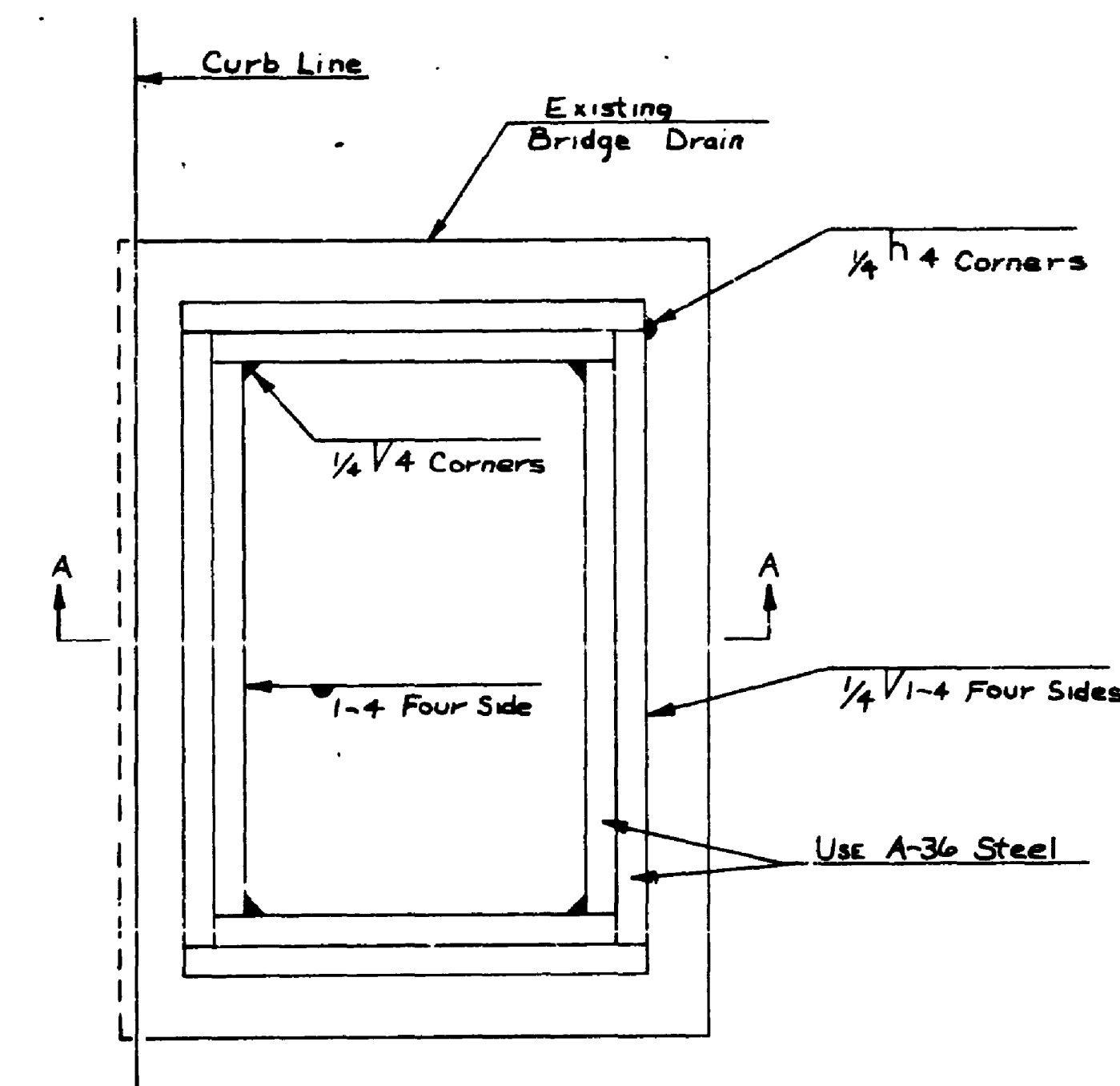
N.E. 145th St. Sta. 0+85.27 to Sta. 3+34.27

- ① High Early Strength Latex Modified Concrete Overlay (1 1/2" Depth)
- ② Scarify Existing Bridge Deck And Approach Slabs (1/4" Depth)
- ③ Asphalt Concrete Pavement Class B (0.12' Comp. Depth)
- ④ Planing (0.02' To 0.12' Depth)

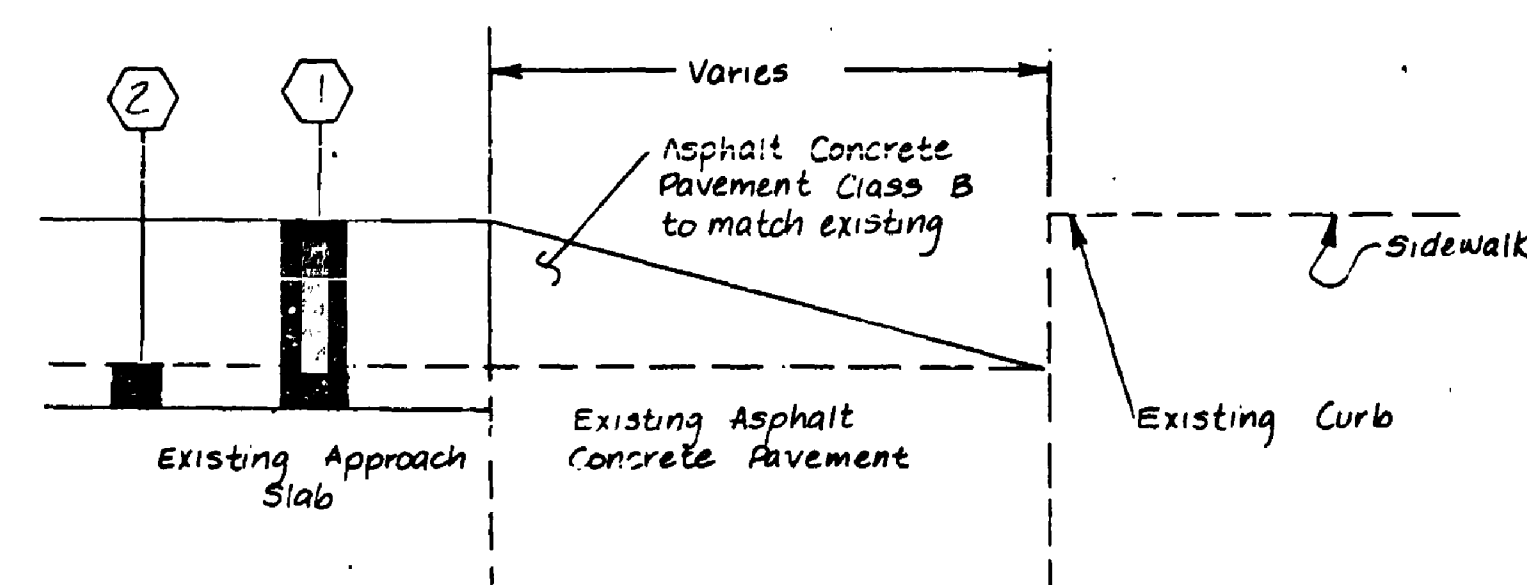


BUTT JOINT AND PLANING DETAIL

N.E. 145th St. Sta. 0+35.27 to Sta. 0+60.27
N.E. 145th St. Sta. 3+59 to Sta. 3+84

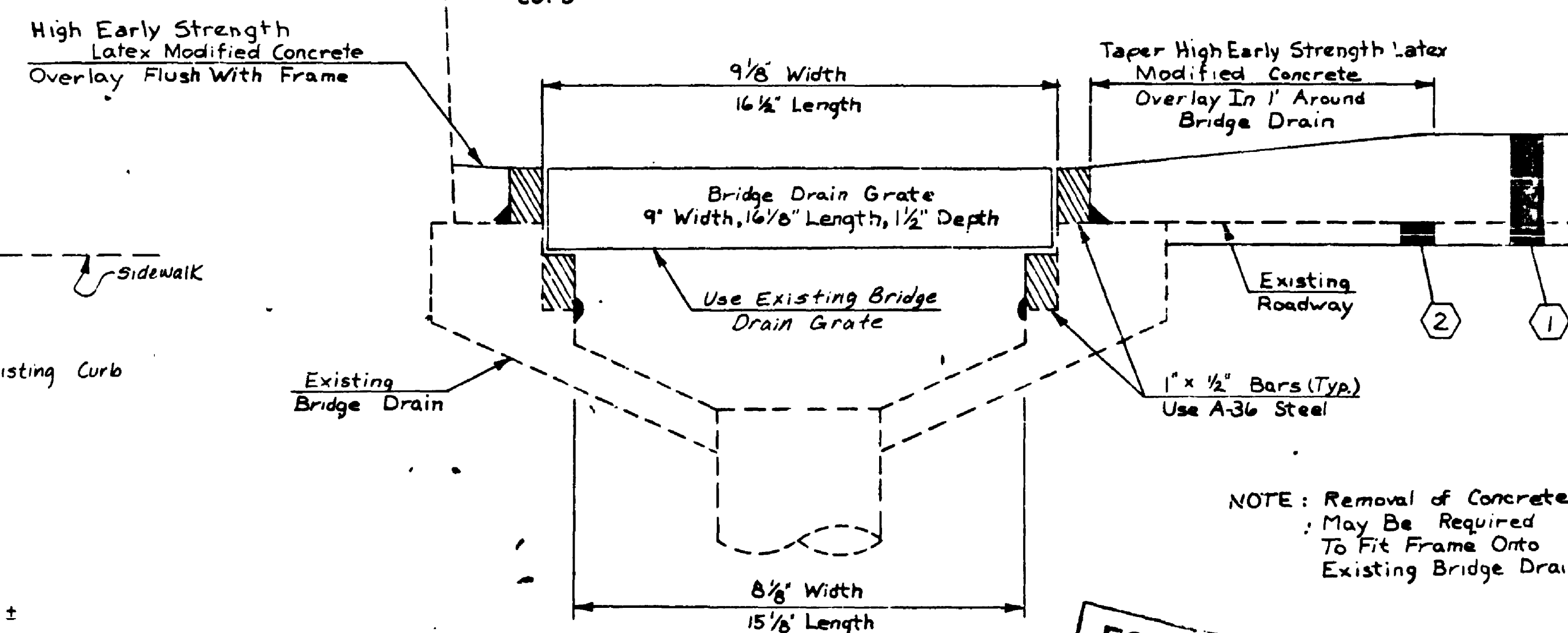


ADJUSTING BRIDGE DRAIN DETAIL



CROSS SECTION OF PAVEMENT TRANSITION DETAIL

N.E. 145th St. Sta. 3+42.5± to Sta. 3+69.5±
N.E. 145th St. Sta. 3+26± to Sta. 3+50.5± Lt (Section Reversed)



SECTION A-A

ADJUSTING BRIDGE DRAIN DETAIL

N.E. 145TH ST. STA. 2+08 (29' LEFT)
N.E. 145TH ST. STA. 2+24 (29' RIGHT)
N.E. 145TH ST. STA. 2+83 (29' LEFT)
N.E. 145TH ST. STA. 2+99 (29' RIGHT)

FOR P.S. INSTRUCTED ONLY

| | | | | | | | | | |
|-------------|---------------|-------|----|--------------------|----------------|--------------|------|--------------|----|
| ENGINEER | DANIEL TAMSKY | STATE | 10 | FED. AID PROJ. NO. | 1-IRG-5-3(622) | SHEET NO. | 24 | TOTAL SHEETS | 24 |
| DRAWN | J.R. Williams | STATE | 10 | JOB NUMBER | 89W041 | CONTRACT NO. | 3643 | DATE | |
| REVIEWER | JOHN KERR | STATE | 10 | BY | APP'D | DATE | | REVISION | |
| CHECKED | HARRY ROLLINS | STATE | 10 | BY | APP'D | DATE | | REVISION | |
| PROJ. ENGR. | S. MILLER | STATE | 10 | BY | APP'D | DATE | | REVISION | |
| DIST. ADM. | R. BOCKSTUCK | STATE | 10 | BY | APP'D | DATE | | REVISION | |

DIVISION OF HIGHWAYS



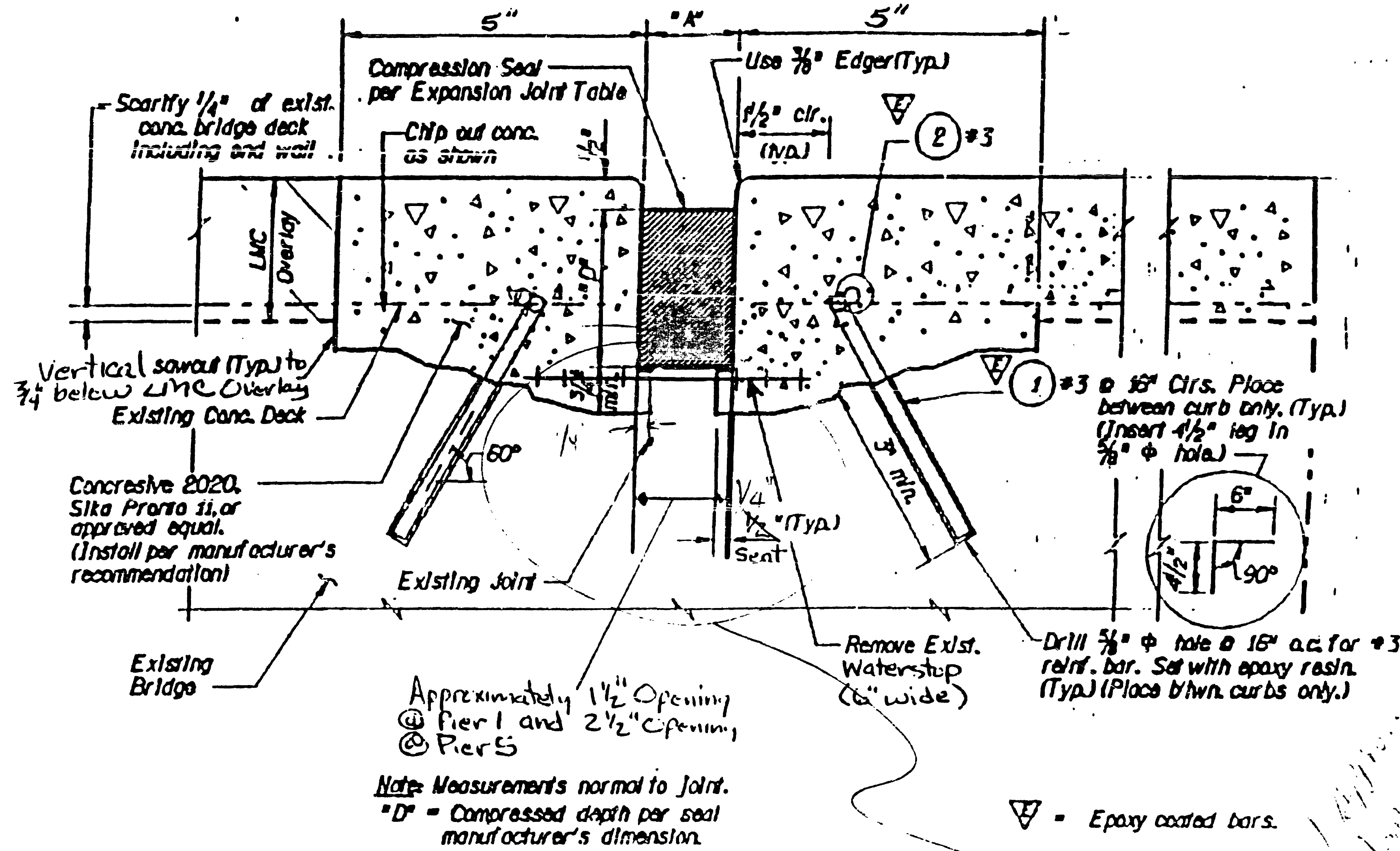
Washington State
Department of Transportation

SR 5

NE 145TH STREET INTERCHANGE

DECK RESURFACING DETAILS

SHEET
24
OF
24
SHEETS



TYPICAL SECTION AT BRIDGE END

| Location | (Normal to Joint) Opening "A" | | | Expansion Jt. Type |
|----------|----------------------------------|--------|--------|--|
| | @ 40°F | @ 60°F | @ 80°F | |
| Pier 1 | 1 3/4" | 1 1/2" | 1 3/8" | ESCO X250C, Watson Bowman & Acme WA D.S. BROWN H-2503 |
| Pier 5 | 2 3/4" | 2 1/2" | 2 3/8" | ESCO X4000, Watson Bowman & Acme WA D.S. BROWN H-4000 |

3643
Co. # 2
Sheet 3 of 4